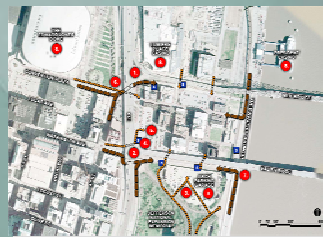


# Laclede's Landing

public realm implementation plan



Laclede's Landing Redevelopment Corporation



# Laclede's Landing

public realm implementation plan



Laclede's Landing Redevelopment Corporation



# Laclede's Landing

public realm implementation plan



## Laclede's Landing Redevelopment Corporation

710 North Second Street  
Suite 310 North  
Saint Louis, MO 63102  
(314) 241-5875

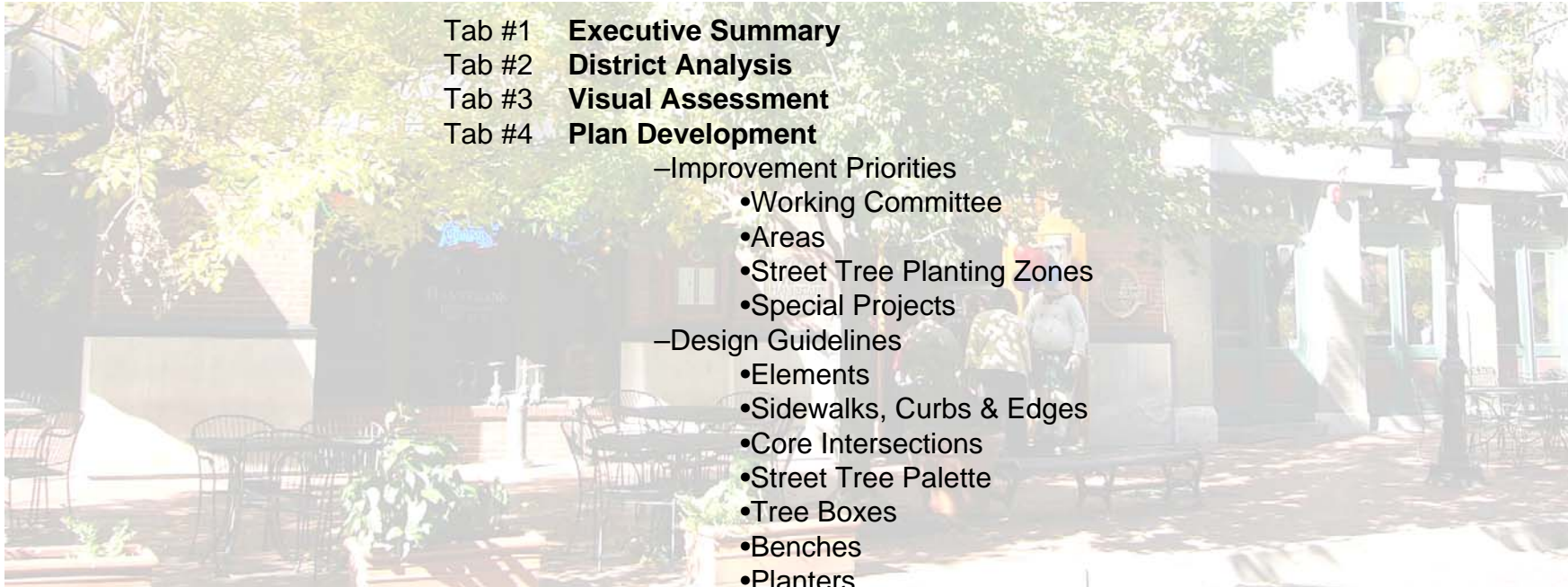


*Prepared by:*

### **Planning Design Studio**

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# Table Of Content



Tab #1 **Executive Summary**

Tab #2 **District Analysis**

Tab #3 **Visual Assessment**

Tab #4 **Plan Development**

–Improvement Priorities

- Working Committee

- Areas

- Street Tree Planting Zones

- Special Projects

–Design Guidelines

- Elements

- Sidewalks, Curbs & Edges

- Core Intersections

- Street Tree Palette

- Tree Boxes

- Benches

- Planters

- Bollards

- Trash receptacles

Tab #5 **Cost**

–Areas and Quantities

–Block Number Location Map

–Summary of Streetscape Cost

–Cost for Typical Streetscape Elements





# Executive Summary



# Executive Summary



## The District

Laclede's Landing is a nine (9) square block neighborhood that is defined by Laclede's Landing Boulevard (north), the Mississippi River (east), Washington Avenue (south) and 3rd Street (west). Within downtown St. Louis, Laclede's Landing is a top visitor destination. It is also surrounded by critical landmarks and destinations: the Gateway Arch, the Mississippi River, Lumiere Casino, Edward Jones Dome, the Central Business District and connected to the Riverfront Trailhead and Washington Avenue.

## Introduction<sup>1</sup>

Laclede's Landing is where downtown St. Louis began with fur trappers and merchants arriving by riverboat to sell and trade their goods. Today it is a major visitor destination (#2 according to St. Louis Downtown Partnership research, October 2008) and the mixed-used neighborhood is occupied by businesses, restaurants, retail, entertainment and service companies. This Plan is intended to make general functional and aesthetic improvement recommendations in the neighborhood to maintain the historical character while providing for long term economic sustainability.

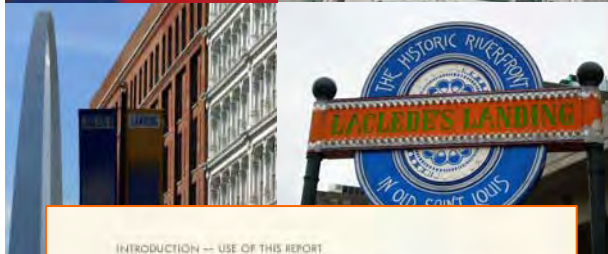
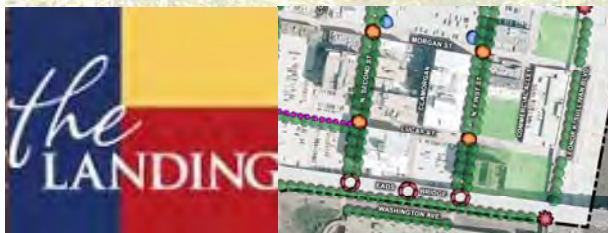
Laclede's Landing is an historic neighborhood founded by Pierre Laclede and Auguste Chouteau in 1763 as a trading post relying on the Mississippi River as the major mode of transportation. Today, while transportation has expanded to include other modern modes, it is more important now than ever that the Landing, provide functional and safe access for those arriving by river, motor vehicle, Metro, bike or on foot.

While the Landing is a top tourist destination, it also serves as a hub to many adjoining attractions. When people visit the Gateway Arch or attend a convention at the Edward Jones Dome, they often also visit the Landing. As of 2007, Metro reported that the Gateway Arch/Laclede's Landing Metro station served an average 1,209 rider per day. Laclede's Landing is a showcase for the St. Louis metropolitan area and it should be cared for and maintained to reflect its history.

<sup>1</sup>(narrative for project justification taken from multiple funding grant applications)



# Executive Summary



## INTRODUCTION — USE OF THIS REPORT

"Urban Design Guidelines" represents a refinement and interpretation of the Laclede's Landing Development Plan as adopted by the City of St. Louis in November 1975.

"Guidelines" has been formulated for use by the following parties: (1) assisting the Planning Committee and the Board of Directors of the Laclede's Landing Redevelopment Corporation in determining policy for a wide range of environmental design issues including — building height/scale, signage/graphics, facade color/materials, and street furniture/fixtures; (2) providing developers, investors, and architects with preliminary design criteria, thus enabling duplication and lowering front-end costs; and (3) assisting the Laclede's Landing Redevelopment Corporation, the City of St. Louis, and similar bodies in developing measures for the function, maintenance, and design of capital improvements.

The choice of the title word "Guidelines" was made with particular care. It is based upon the recognition that rigid controls or predetermined rules do more harm than good when applied to complex renewal undertakings of this nature. The ultimate environmental quality must be determined by factors in addition to this document. These include encouragement of capable design professionals and application of a "design review" function. Particular scrutiny shall be necessary regarding the challenge of integrating new architectural features into a pre-established historical context. Ghivardell Square, Society Hill and numerous other successful projects have already proven the efficacy of such efforts.

Although "Guidelines" is a semi-technical document, every effort has been made to simplify the format in order that it be as easy to read as skin is possible. It is bound in a loose-leaf notebook so that any portion can be easily removed or replaced. The use of color throughout is intended to add readability and is not to be confused with the actual guidelines. Whenever possible, individual Goals are confined to no more than one page and include a brief discussion followed by a specific recommendation or example. Goal groupings fall under common Topic headings. Topics are organized under each of the major divisions or parts of the report, each Part containing material of primary interest to one segment of the intended audience (i.e., Part 3 is for private developers). Not only does the "Urban Design Guidelines" function as a compilation of over 100 important design issues, but it is one of the basic corporate documents to be used by the Redevelopment Corporation in decisions affecting the revitalization of a treasured urban resource — Laclede's Landing.

## The Plan

The intent of this document is to update and enhance the Design Guidelines created and published in 1976. These updated public realm guidelines support the previously established design themes and guidelines for the District. Overtime of course, various elements need updating, key design elements need to be identified and new goals established.

For general guidance, a mission statement and project goals were established for the plan.

## Mission Statement

The mission of the Laclede's Landing Public Realm Implementation Plan is to strengthen the Landing's image as a pre-eminent visitor destination, high quality workplace featuring a vibrant mix of activities, historic preservation and engaging public spaces.

## Project Goals

- Create a public environment which supports the economic vitality of the Landing
- Provides mobility for its users
- Creates memorable and interesting experiences
- Reflects the scale, pattern and historical context of the area
- Create a vision for a well balanced environment
- Develop a desirable image and identity for the Landing
- Provide a unified streetscape to support public and private projects
- Balance the mobility of visitors with the access needs for vehicular traffic, pedestrians and service providers.



# Executive Summary



## District Analysis

In order to formulate the plan updates, several initial efforts were conducted to identify deficiencies and establish priorities. **Tab #2** contains a **District Analysis**, several summary analysis documents that define the Landing's components. A **Current Conditions** summary identifies basic opportunities and deficiencies on the Landing. A **Contextual Analysis** identifies vehicular and pedestrian connection points into and out of the District. The opportunities for expanded connectivity to the Landing and first impressions were the primary reasons for conducting this analysis. The **Functional Conditions** analysis identifies key components and conditions that exist in the District that add or detract from the Landing experience.

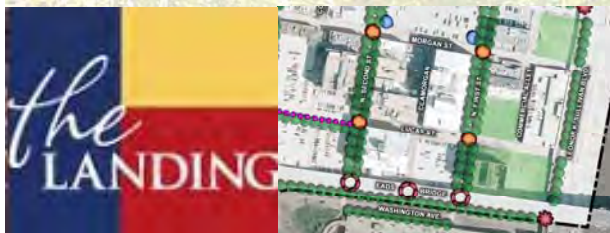
## Visual Assessment

**Tab #3** contains a **Visual Assessment** of the District. The Visual Assessment identifies the streetscape elements that are positive or negative; how they affect the visual environment; and general guidance for upgrades. A Visual Assessment was completed for each block face in the District.





# Executive Summary



-  Street Tree Planting Zones
-  Pedestrian Access Through Parking Lot
-  Pedestrian Plazas
-  Portal Signs

## Plan Development

The Plan Development (**Tab #4**) contains the priority elements established during the planning process. This section provides recommendations on the most compelling elements that require the highest level of attention. Factors for establishing the priorities include (1) affordable aesthetic enhancements; (2) pedestrian safety and comfort; (3) significant projects that when implemented will make a distinctive difference to the visual environment; (4) selecting “doable” projects for demonstration of what can be accomplished within the fiscal resources of the District.

The Plan Development section contains details of the Washington Street Beautification Project, along Washington from Memorial Drive to Leonor K. Sullivan Blvd.

The Plan Development contains guidance on the following elements:

- Improvement Priorities
  - Working Committee
  - Areas
  - Street Tree Planting Zones
  - Special Projects
    - Portal - First St. at Washington St.
    - Lucas Pedestrian Extension through parking lot
    - Building Sign Painting
    - Bridge Portal Sign
- Design Guidelines
  - Elements
  - Sidewalks, Curbs & Edges
  - Core Intersections
  - Street Tree Palette
  - Tree Boxes
  - Benches
  - Planters
  - Bollards
  - Trash Receptacles
- Washington Street Beautification Plan

# Executive Summary



## Cost

**Tab #5** contains a generalized **Cost** budget level estimate for all improvements throughout the District. The first step in determining the level of effort required, consisted of measuring each block face in the District. The District contains approximately 12,500 lineal feet (LF) of block face.

Each Block face was then classified as to its need for repair/renovation as either: (1) Maximum, (2) Moderate or (3) Minimum cost requirement. Each Classification was represented by a typical block within the District.

A list of improvements for each of the three representative blocks was prepared, quantity take-offs completed and unit cost were applied. The result was a cost of general improvements for that representative block. Each of the costs were then applied to the (previously mentioned) block face classifications and LF measurements.

The unit costs were based on mid-2009 prices and do not include escalation factors.

The total estimated cost of Public Realm Streetscape improvements is approximately \$4.17 million (does not include street improvements).

### SUMMARY OF STREETScape COST

Cost<sup>1</sup> is based on Cost Classification Level Cost<sup>2</sup> X LF Quantity of Streetscape

Block <sup>3</sup> That Require:	Cost Class		Cost <sup>2</sup>
	Total LF <sup>1</sup>	Unit Cost <sup>2</sup>	
<b>A. Maximum</b> Redevelopment (Based on Block 25-Block Face E)	6409	\$ 335.53	\$ 1,954,550.49
<b>B. Moderate</b> Redevelopment (Based on Block 15-Block Face N)	4056	\$ 212.58	\$ 863,758.88
<b>C. Minimum</b> Redevelopment (Based on Block 68-Alley)	1434	\$ 100.26	\$ 143,769.26
<b>Other Costs</b>			
<b>D. Portal Signs, Building Sign Painting, Core Intersection Treatment</b>	na	1.5	\$ 400,000.00
<b>Subtotal Construction Cost</b>	12499		\$ 3,498,068.59
<b>Professional Services Fees</b>			
Topographic, Property & Easement Surveys			\$ 30,000.00
Geotechnical/Soils Investigation - Allocation if needed			\$ 15,000.00
Design, Engineering & Construction/Bid Docs	10%		\$ 349,806.86
Design Team Construction Period Services	3%		\$ 104,942.60
Construction Management & Support Services	5%		\$ 174,903.33
<b>Subtotal Professional Services</b>			\$ 674,655.59
<b>Total Cost</b>			\$ 4,172,724.18

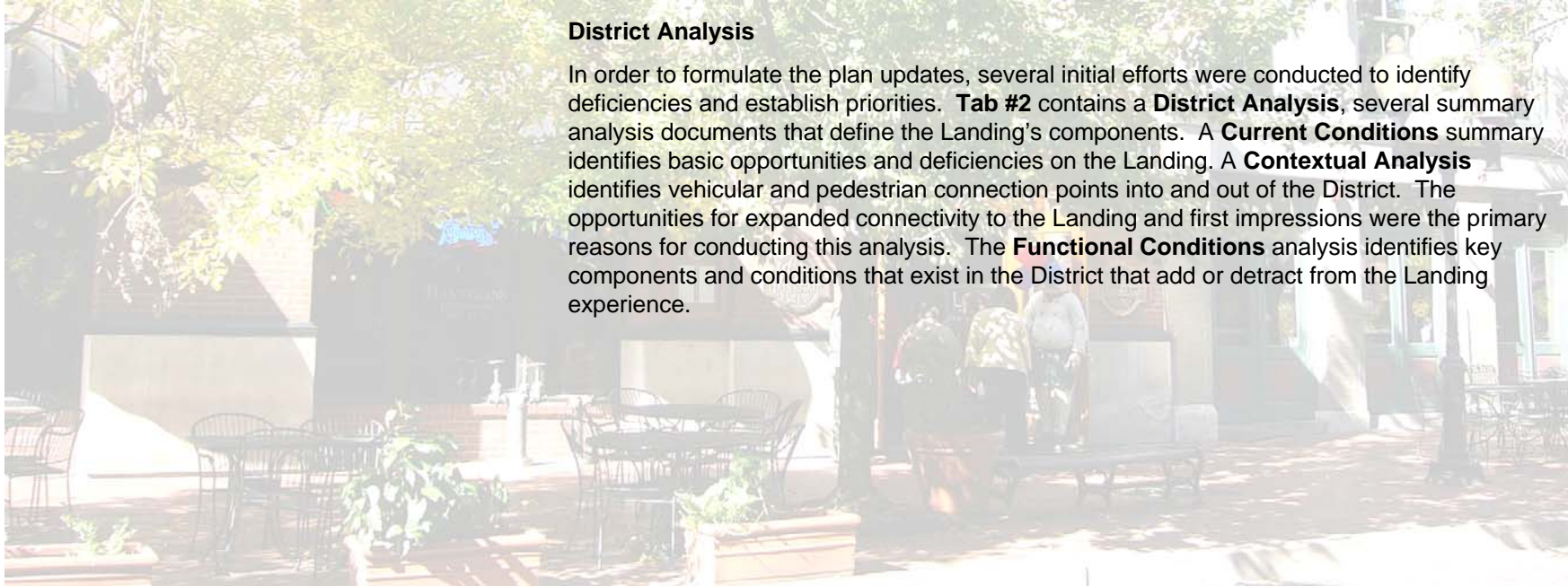
Costs are conceptual general order of magnitude and do not contain an escalation factor





# District Analysis

# District Analysis



## District Analysis

In order to formulate the plan updates, several initial efforts were conducted to identify deficiencies and establish priorities. **Tab #2** contains a **District Analysis**, several summary analysis documents that define the Landing's components. A **Current Conditions** summary identifies basic opportunities and deficiencies on the Landing. A **Contextual Analysis** identifies vehicular and pedestrian connection points into and out of the District. The opportunities for expanded connectivity to the Landing and first impressions were the primary reasons for conducting this analysis. The **Functional Conditions** analysis identifies key components and conditions that exist in the District that add or detract from the Landing experience.



# Location Map



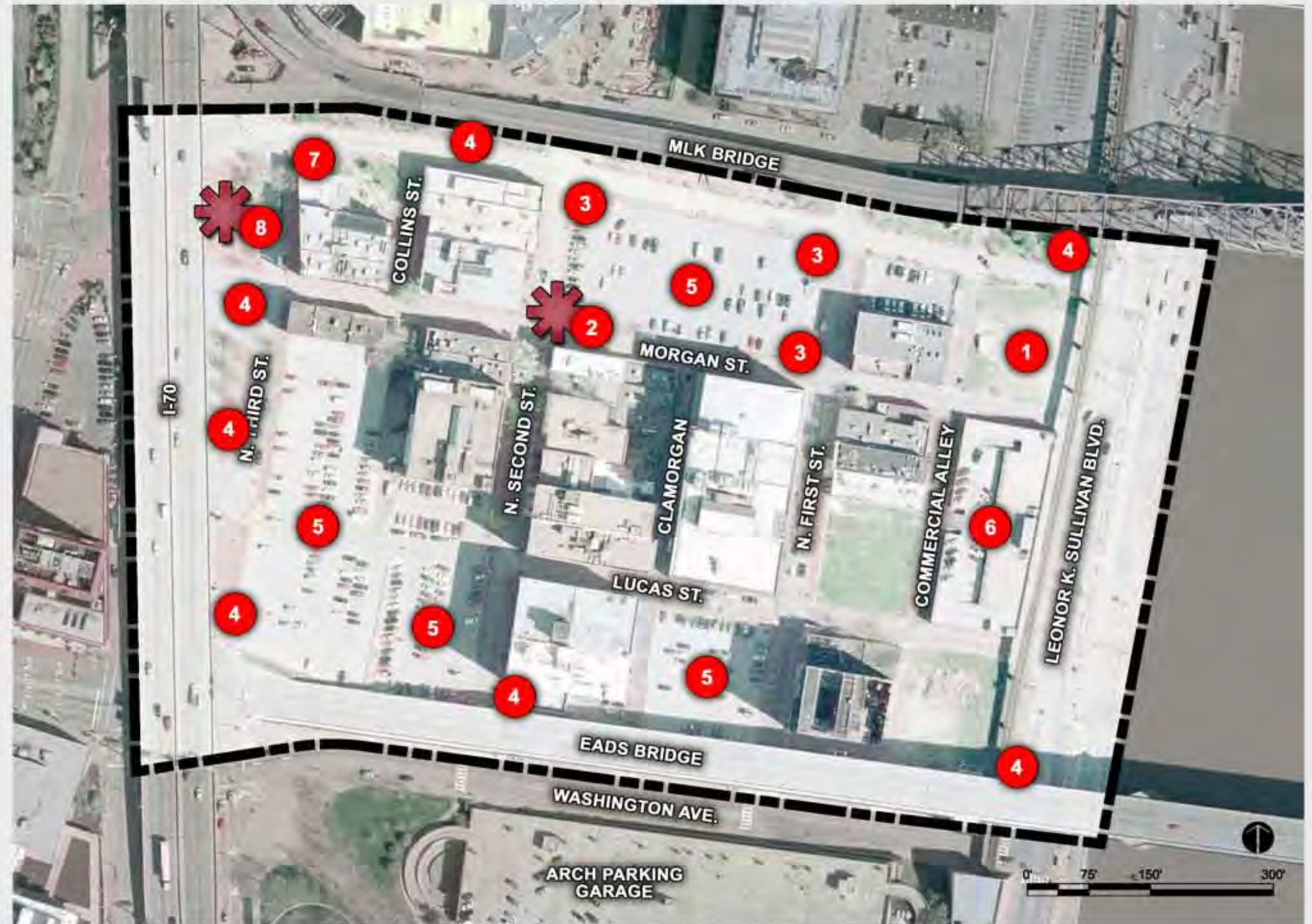
Legend





# Current Conditions

## Key Elements

1. Proposed observation wheel location.
2. Moll's Clock Plaza – Nice pedestrian space accentuated by the historic clock. Street furnishings need replacement, along with additional benches.
3. Parking lot corner pedestrian spaces. Updated street furnishings and planting needed at various locations.
4. Public Art – Several pieces need renovation or replacement.
5. Public Parking – All parking lots need planting islands and accommodations for pedestrian circulation. Improvements to attendant booths also needed.
6. Riverfront Parking Garage – Needs permanent secondary flood access route. New top deck treatment with consistent period lighting and flagpoles with uplighting also needed.
7. Ladue Garden Club Pocket Park – Needs renovation and on-going attention to continue serving as a green space.
8. Existing Public Fountain – Needs to be renovated, or removed. A non-functioning fountain in this location projects a poor first impression.



### Legend

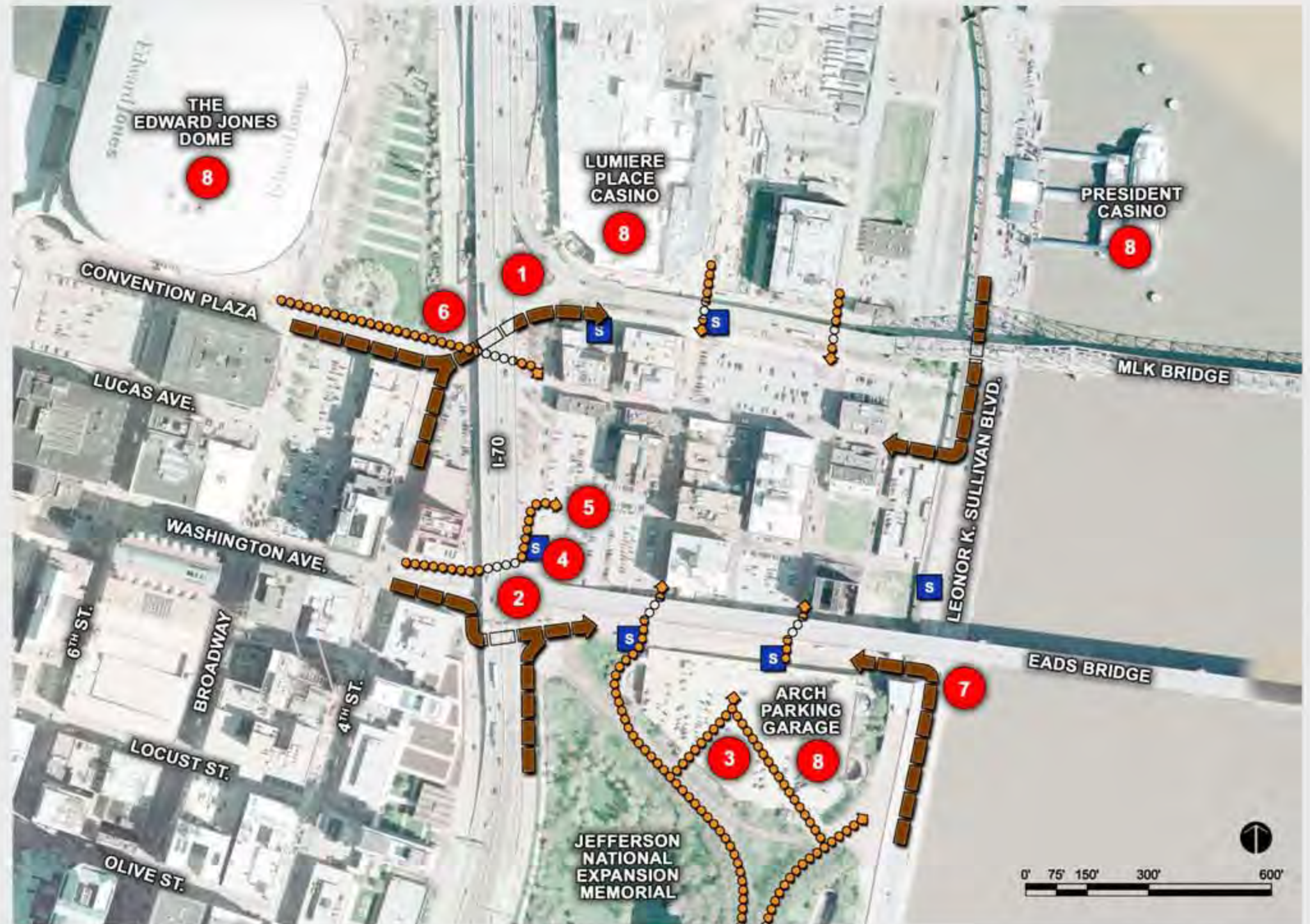
-  Laclede's Landing Boundary
-  Existing Landmarks






# Contextual Analysis

## Key Elements

1. Vehicular access into the Landing from the northwest and I-70 is very difficult.
2. Vehicular and pedestrian access from Washington Ave. is difficult due to I-70 bridge/overpass configuration.
3. Visitor orientation and pedestrian access to the Landing from the Arch Grounds is difficult, due to the Arch Garage which blocks visual connections, and requires pedestrians to circulate through the garage structure.
4. Important and most visible corner of the Landing lacks identification signage. The casino's digital billboard draws immediate attention.
5. Pedestrian connection to the Landing from the southwest requires that people walk through a parking lot, resulting in a poor first impression.
6. Poor visibility at night into the Landing from this location.
7. No identification of the Landing from this important vehicular entry point.
8. The Edward Jones Dome, Lumiere Casino, President Casino, and Arch Parking Garage are substantial activity generators which the Landing benefits. Maintaining good access to these is of vital importance.



### Legend

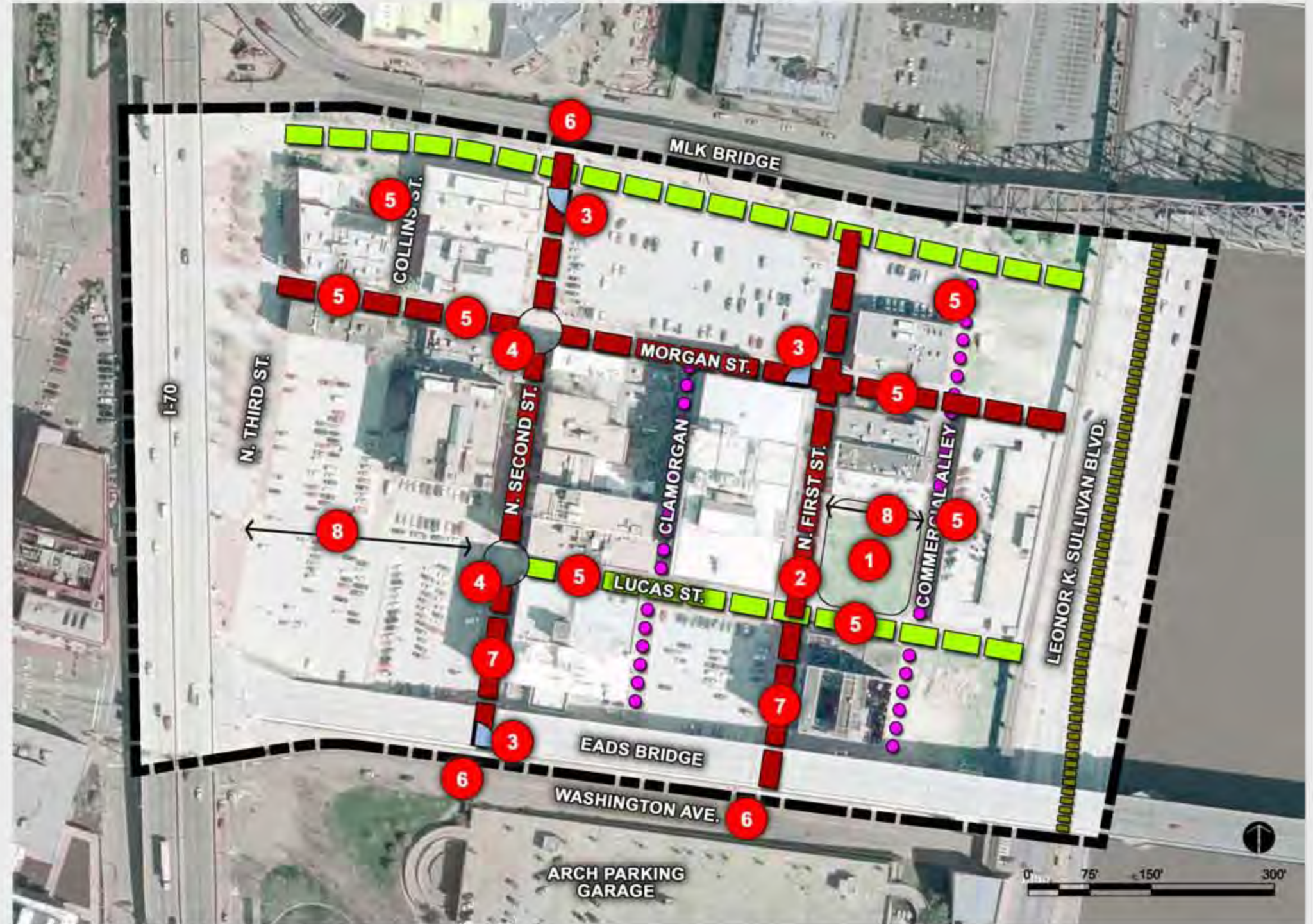
-  Pedestrian Access Route
-  Vehicular Access Route
-  LaClede's Landing Marquee Sign



# Functional Systems

## Key Elements

1. Private land which is used as an outdoor amphitheater venue for the various music festivals. If developed in the future, a new location will be needed.
2. Cobblestone streets are a major contributor to the historic character of the Landing. However, their condition has deteriorated and is in need of major repair.
3. Traffic calming gates reduce gridlock during periods of peak traffic volume. Gates should be painted black to blend in with other street furnishings.
4. Big Muddy Blues Festival stage venue locations, which require the closing of Second Street.
5. Function of sidewalk constrained by light poles, tree grates, and/or sign poles located in the center of the sidewalk.
6. Location of significant pedestrian traffic into the Landing area.
7. Emergency service and access routes.
8. Informal pedestrian circulation route.



### Legend



Laclede's Landing Boundary  
 Major Vehicular Traffic  
 Minor Vehicular Traffic



Major Service Alley  
 Mississippi River Bike Trail  
 Existing Vehicular Gates



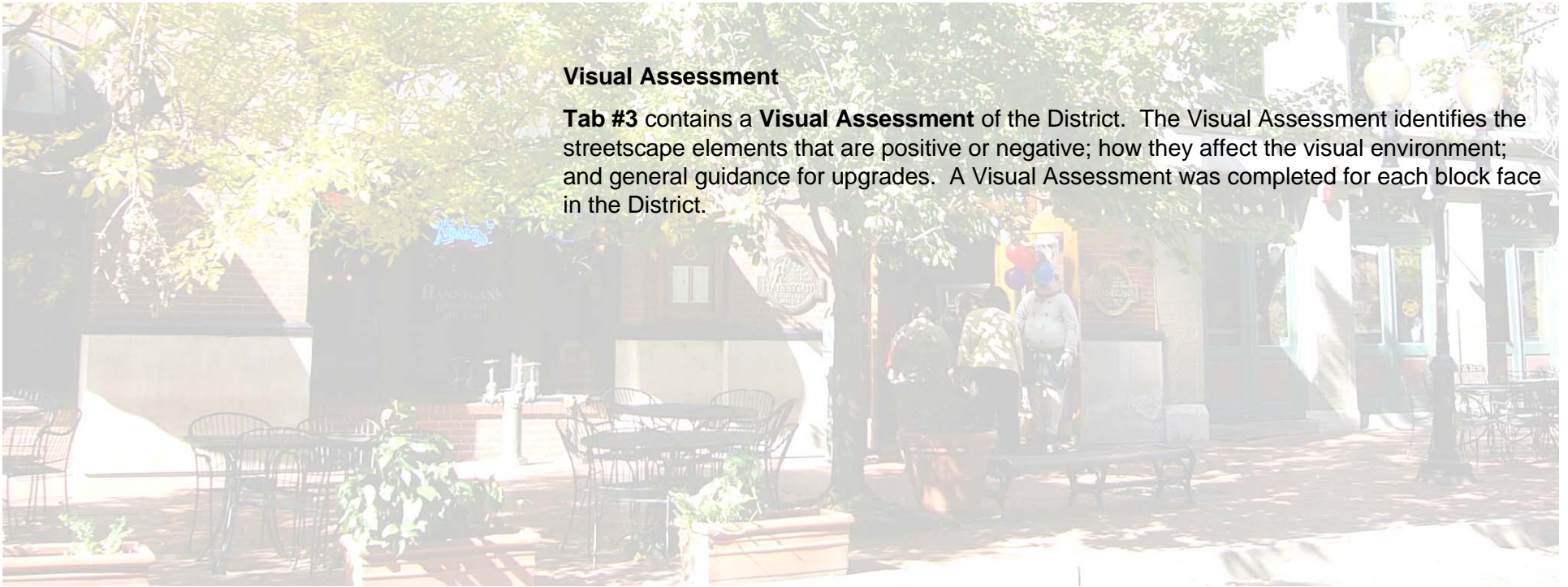


# Visual Assessment

# Visual Assessment

## Visual Assessment

**Tab #3** contains a **Visual Assessment** of the District. The Visual Assessment identifies the streetscape elements that are positive or negative; how they affect the visual environment; and general guidance for upgrades. A Visual Assessment was completed for each block face in the District.





# Visual Assessment

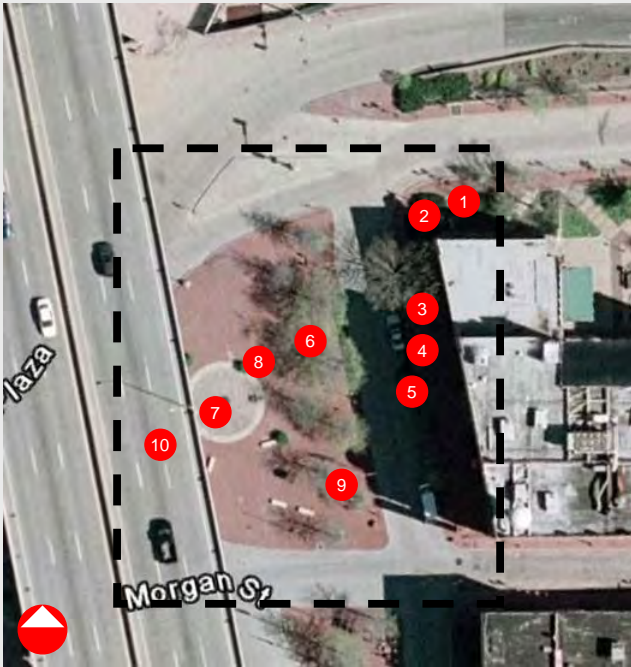
## Block 68W Third St.



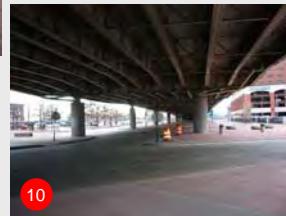
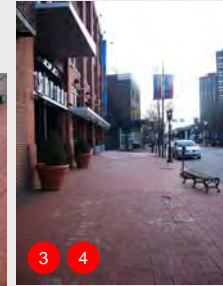
Key Map

### Legend

1. Laclede's Landing marquee sign difficult to see in this location.
2. Ladue Garden Club pocket park needs on-going attention to reestablish it as an open space feature for users of the Landing.
3. Pedestrian pavement uneven and needs to be reset and benches need to be replaced.
4. Large open areas of sidewalk pavement present opportunities for outdoor dining in the future.
5. Trees which have been removed need to be replaced.
6. Pavement near existing mature trees needs need to be reset.
7. At-grade fountain is a nice feature, however it does not currently function. It should be renovated for use during the summer months or replaced with a different feature.
8. Adjacent fountain equipment housing needs to be repaired, replaced and/or relocated.
9. Existing directory needs to be moved to a more prominent location.
10. Area under I-70 overpass is unexciting and should be visually appealing to draw people into the Landing from Downtown.



Site Map



# Visual Assessment

## Block 68 Laclede's Landing Blvd.



Key Map

### Legend

1. Concrete island should have brick pavers and planters installed in the future.
2. Potential area to relocate existing Laclede's Landing marquee sign.
3. Island planting should be renovated including demo of damaged ornamental fence.
4. Light pole base needs to be reset.
5. Planting area needs major renovation.
6. Newspaper box could be combined with existing mailbox in this location.
7. Corner walk is very narrow.
8. Cut off metal fence posts are a tripping hazard and should be removed.
9. Large trees block sidewalk.
10. Treatment of space under bridge should match treatment in Kassabaum Park with the exception of the fence.
11. Street trees block very narrow sidewalk.
12. Planter wall and trees in grates block walk. Consider removal of planter against building.



Site Map





# Visual Assessment

## Blocks 68W & 68E Collins Alley



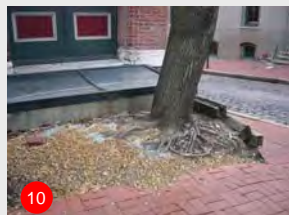
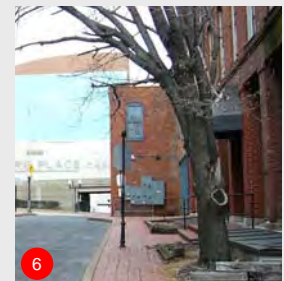
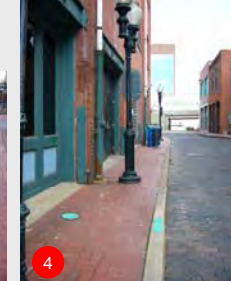
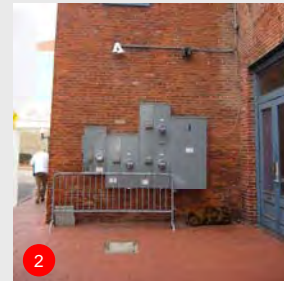
Key Map



Site Map

### Legend

1. Corner sidewalk very narrow due to tight radius. Uneven pavement makes access difficult.
2. Electrical panels on wall are exposed and need screening.
3. Open planting beds should be replanted.
4. Existing lights block a very narrow sidewalk.
5. Exposed trash can should be stored within building envelope.
6. Sign pole blocks sidewalk.
7. Brick sidewalks have settled and need to be replaced.
8. Wood retaining walls failing and should be replaced.
9. Pavement around existing manhole has settled or is missing causing tripping hazards and effectively blocking the sidewalk.
10. Existing mature tree has overgrown planting area. A new planting area should be constructed to provide additional room for tree, or tree should be replaced.
11. Pavement around utility box has settled causing drainage problem and tripping hazard.



# Visual Assessment

## Block 68E & 25 Second St.



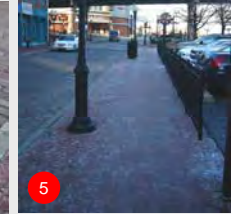
Key Map

### Legend

1. Corner sidewalk restricted due to obstructions and slope.
2. Existing traffic gate should be painted black to match other furnishings in the area.
3. North Pedestrian Node assessment:
  - Create larger planting area around existing tree.
  - Provide shrub planting for buffering against existing parking lot.
  - Provide planting strip to resolve grade issue at edge.
  - Provide benches.
4. Cobbles in roadway need to have asphalt patches removed and stone reset.
5. Street trees needed in this section of sidewalk.
6. Mature tree needs to have large girdling root removed and a larger planting area installed with adjacent pavers reset.
7. Existing electric meter in parking lot needs to be screened from view.
8. Uneven pedestrian paving at tree planting areas needs to be reset.
9. Street tree plantings needs to be replaced.
10. South Pedestrian Node assessment:
  - Replace shrub plantings and trees.
  - Provide shrub planting for buffering against existing parking lot.
  - Remove unused newspaper box.
  - Replace benches.
11. Circular concrete band needs replacement and uneven intersection pavers need resetting.



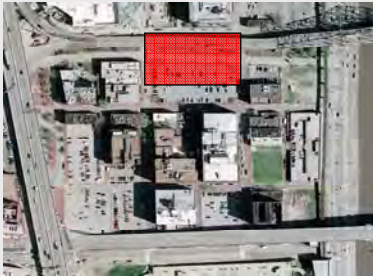
Site Map





# Visual Assessment

## Block 25 Laclede's Landing Blvd.



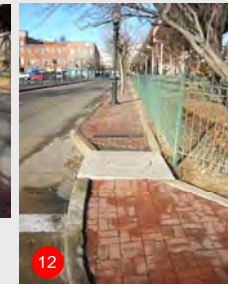
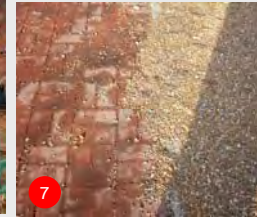
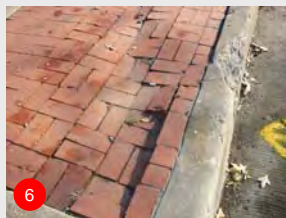
Key Map



Site Map

### Legend

1. Light pole blocks walk at ramp.
2. Non standard street sign needs to be replaced to match others in Landing.
3. Transition between asphalt surface and cobbles needs to be created with a concrete band.
4. Laclede's Landing marquee sign is hard to see under bridge.
5. Large trees have heaved sidewalk pavement and block narrow sidewalk.
6. Minor pavement reset necessary in this area.
7. Kassabaum Park - Decorative metal fence helps visually establish character of the park.
  - Additional benches should be provided for users.
  - Pea gravel is a nuisance and should be replaced with smaller compacted rock with fines.
  - Add additional trash cans to the park.
8. Blank sign post should be utilized.
9. Parking lot fence has temporary supports, and will need to be reattached in the future.
10. Curb stops at edge of parking lot are unattractive and should be replaced with curbing.
11. This portion of the parking lot is higher in grade than surrounding walk. Paved slope solution is unattractive.
12. Sidewalk blocked by trees and lights located in the center of walkway.



# Visual Assessment

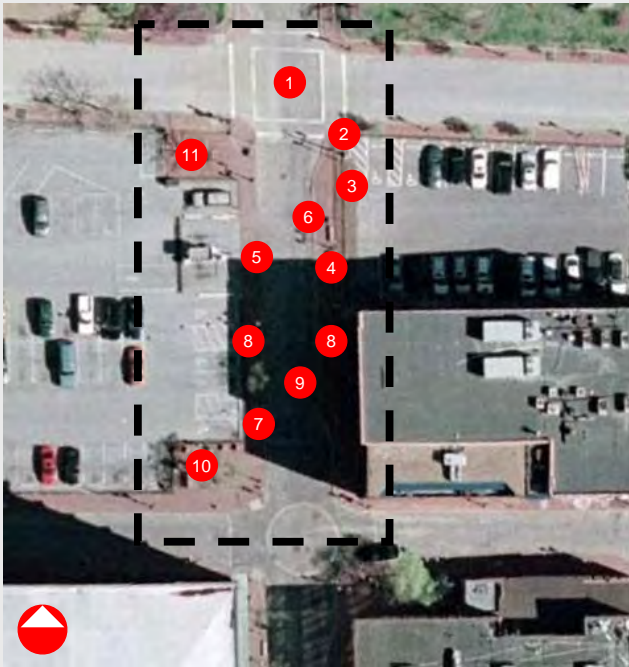
## Blocks 25 & 16W First St.



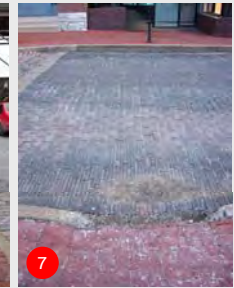
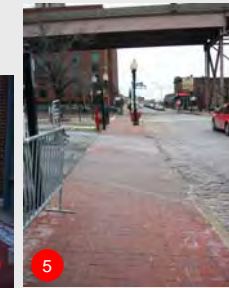
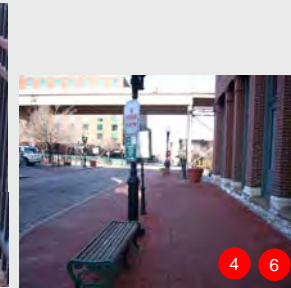
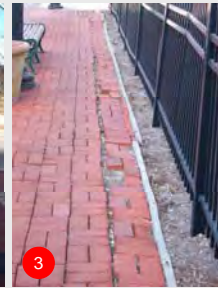
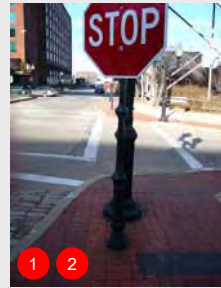
Key Map

### Legend

1. Laclede's Landing Blvd. is only concrete street in the landing.
2. Steep ramp does not meet current ADA guidelines.
3. Replace brick pavement header curb.
4. This section of the streetscape environment lacks planting. It has width to accommodate additional landscape planting
5. Curbing at parking lot entrance needs replacement. Drive apron creates steep cross slope with does not meet ADA guidelines.
6. All benches along street need replacement. Street trees are needed on both sides of street.
7. Handicap ramp needed on this side of street opposite existing ramp.
8. Sidewalk brick needs to be reset on both sides of street.
9. Street surface very uneven. Cobble needs to be reset.
10. North Pedestrian Node assessment:
  - Create larger planting area around existing tree.
  - Provide shrub planting for buffering against existing parking lot.
  - Remove small trash can and replace with larger units.
11. South Pedestrian Node assessment:
  - Create larger planting area around existing tree.
  - Add shrub planting against existing fence.
  - Reset uneven brick pavers.
  - Install new benches.
  - Add a small curb wall to create grade transition.



Site Map





# Visual Assessment

## Blocks 16E & 16W Commercial Alley



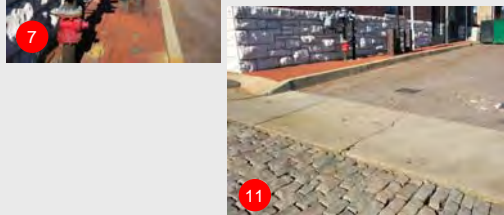
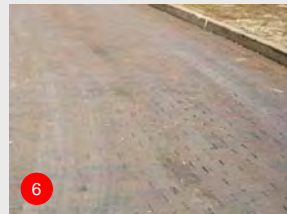
Key Map

### Legend

1. None compliant curb ramp, due to steepness, settlement and narrow width.
2. Brick settlement at curb ramp.
3. Square bollards should be repainted or replaced.
4. Open access doors should be secured with locks.
5. Dumpsters and trash cans in alley should be relocated inside the building envelope.
6. Paving generally in good shape, only minimal spot repairs needed.
7. Light fixtures and bollards block a very narrow sidewalk making it almost unusable.
8. Bollards adjacent to existing light pole need to be repainted, reset, or replaced.
9. Paving around bollard mounting plates needs to be reset.
10. Non ADA compliant ramp in poor condition.
11. Large concrete band needs repair.
12. Paving around existing manholes needs to be reset.



Site Map





# Visual Assessment

## Block 16 Laclede's Landing Blvd.



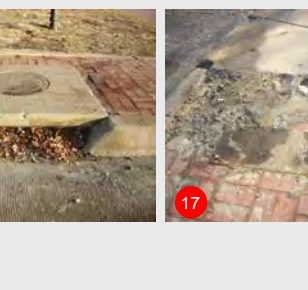
Key Map



Site Map

### Legend

1. Ground mounted street sign missing.
2. Inlet/curb needs repair.
3. Transition between asphalt surface and cobble needs to be created with a concrete band.
4. Street cobble need to be reset.
5. Sidewalk corner ramp very steep and does not meet current ADA requirements.
6. Space below bridge needs to be developed in more useable outdoor space. Consider adding benches and other pedestrian elements.
7. Metal bridge structure should be painted black to harmonize with other elements on the Landing
8. Trees in tree grates block existing walk which is narrow.
9. Trees located in narrow walk adjacent to existing parking lot will be difficult to keep in this area.
10. Inlet and street sign create tripping hazard at sidewalk corner.
11. Wood retaining wall needs to be replaced.
12. Pavers at ramp need to be reset.
13. Cable and bollard barriers are ineffective and should be replaced with some other type of temporary barrier.
14. Graffiti should be removed from existing light pole.
15. Pohrer Park
  - Turf on ship berm is failing and starting to erode.
  - Signs on north side of park need to be straightened.
  - This area has potential as a future outdoor music venue.
16. Inlet need to be cleaned out.
17. Erosion within Pohrer Park, causing sidewalk to be mud covered.





# Visual Assessment

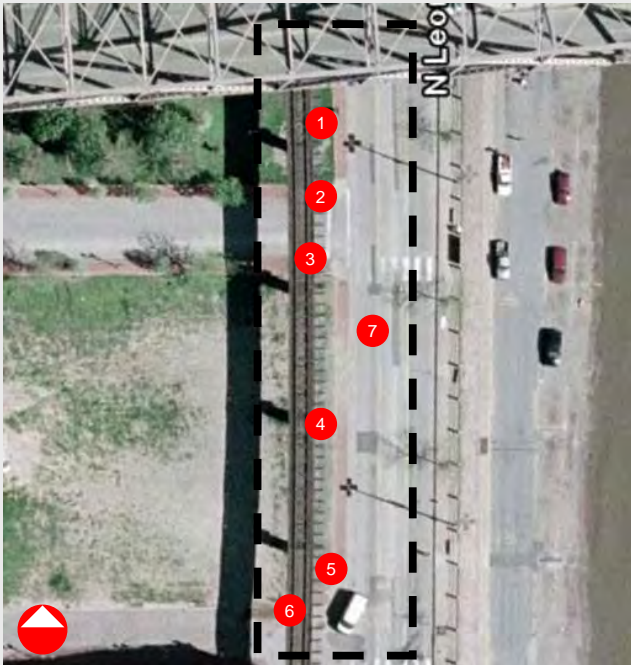
## Block 16E Leonor K. Sullivan Blvd.



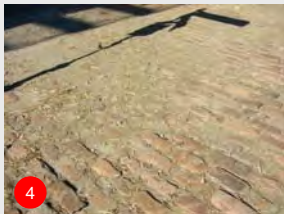
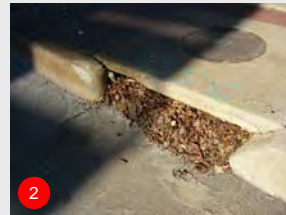
Key Map

### Legend

1. No cobblestone treatment in R.O.W. of this area. It should be installed for consistency and to improve the overall image of the area.
2. Storm inlet is clogged with debris and should receive regular maintenance.
3. Lack of trestle bird barrier is contributing to excess pigeon refuse on area sidewalks.
4. Adjacent vacant land erosion is depositing soil and debris across sidewalks and cobblestones, restricting pedestrian circulation.
5. Sign pole needs to be straightened and repainted.
6. Unsightly erosion problem, creates a poor first impression for visitors who arrive by the riverfront.



Site Map





# Visual Assessment

## Block 67W Third St.



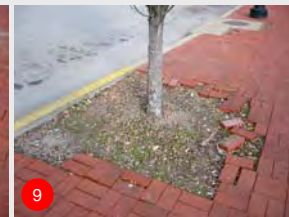
Key Map

### Legend

1. Remove metal guards which have grown into mature trees.
2. Public art and flag pole grouping creates an entry feature to the Landing.
3. Old St. Louis Riverfront wall mural should be restored or replaced with a similar mural featuring the Landing.
4. Stop sign needs to be reset straight.
5. Existing tree bosque creates a nice pedestrian space. Area could be utilized as outdoor dining space for nearby restaurants.
6. Area lacks benches and trash cans.
7. Parking meters should be painted black to match other furnishings in the area.
8. Pavement at existing inlet needs to be reset.
9. Pavement around existing mature trees needs to be reset.
10. Existing section of concrete curb needs to be replaced.
11. Area under I-70 overpass is unexciting and should be visually appealing to draw people into the Landing from Downtown.
12. Public art located in a good highly visible location.



Site Map





# Visual Assessment

## Blocks 67 & 68 Morgan St.



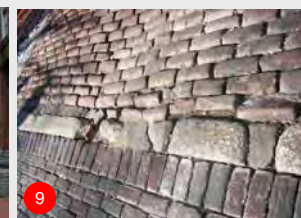
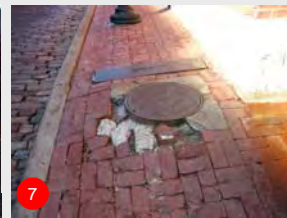
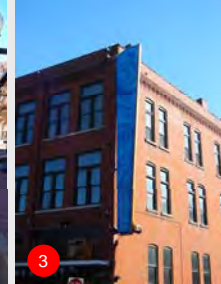
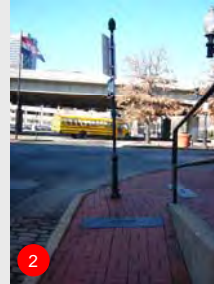
Key Map

### Legend

1. Transition between asphalt surface and cobbles needs to be strengthened with a concrete band.
2. Tight corner due to ramp and sign pole.
3. Marquee sign mounted on building has good visibility.
4. Wider sidewalk in this area could accommodate street trees in the future.
5. Sidewalk pavers missing at various locations.
6. Sidewalk very narrow and restricted by light poles installed in center of walk.
7. Missing pavers at manhole create tripping hazard.
8. Grade into alley restricts ADA access of walkway.
9. Cobble stones very uneven and need to be reset.
10. Bricks around utility box missing and need to be reset.



Site Map





# Visual Assessment

## Blocks 67W & 67E Second Street Alley.



Key Map

### Legend

1. This portion of the alley has been converted into an outdoor dining patio with tables.
2. Concrete retaining wall is failing and should be replaced.
3. Exposed dumpster should be located within the building envelope.
4. Alley is permanently blocked by decorative metal/wood/bamboo fence.
5. Concrete retaining wall with chain link fence which is leaning and should be replaced with fence matching other parking lots. Alley is also blocked by temporary fence barrier.
6. Exposed service dock with storage drums should be screened.
7. Cobra head light fixture mounted on wood pole does not match historic character of other fixtures in the Landing.
8. Dumpster located in alley. Needs to be relocated within building envelope.
9. Make shift yellow handrail should be replaced or repainted black to match other site furnishings.
10. Alley cobbles in poor condition. Need to be reset. Area is used as parking spaces which blocks access.



Site Map



1



2



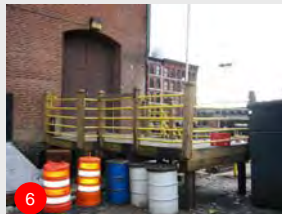
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4



5



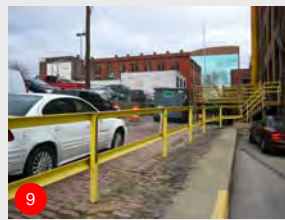
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7



8



9



10



# Visual Assessment

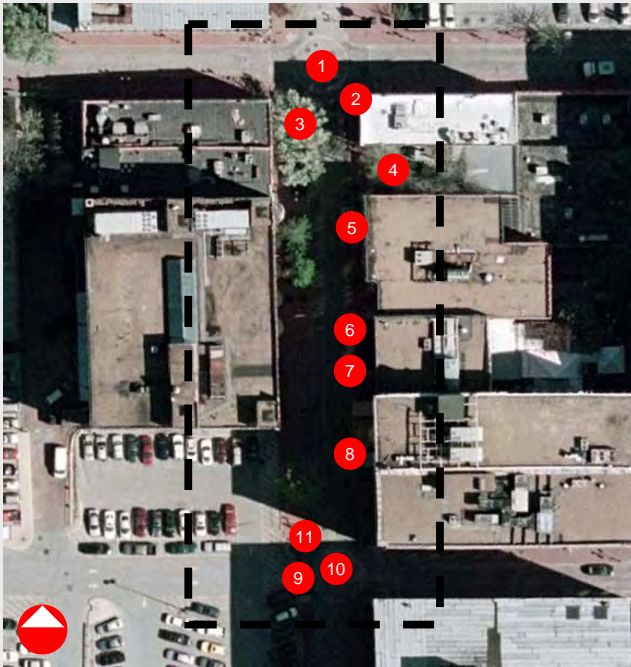
## Blocks 67E & 26W Second St.



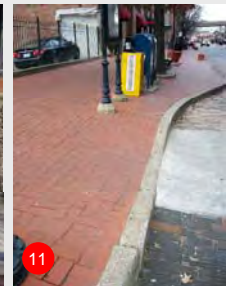
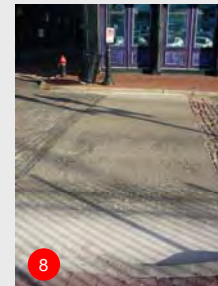
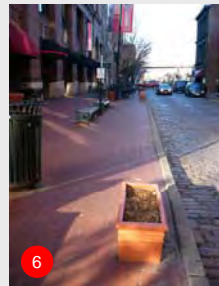
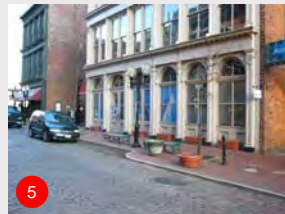
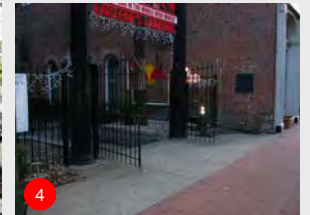
Key Map

### Legend

1. Intersection paving very uneven. Cobbles need to be reset.
2. Steep and narrow corner ramp does not meet current ADA guidelines. Curbing also needs to be replaced.
3. Nice overhead canopy trees create a very desirable outdoor sidewalk/dining environment.
4. Mesker Park nice outdoor space however, it has been incorporated into Big Daddy's Bar.
5. Replacement trees are needed on this side of street.
6. Planters could be utilized to create a small outdoor dining area in this location.
7. Benches need to be replaced in this location. Signs should be straightened.
8. Handicap ramp needed on this side of street opposite existing ramp.
9. Opportunity for creating a pedestrian linkage through parking lot utilizing existing Lucas Ave. R.O.W.
10. Intersection paving very uneven. Cobbles need to be reset.
11. Steep handicap ramp.



Site Map





# Visual Assessment

## Blocks 26W & 26E Clamorgan Alley



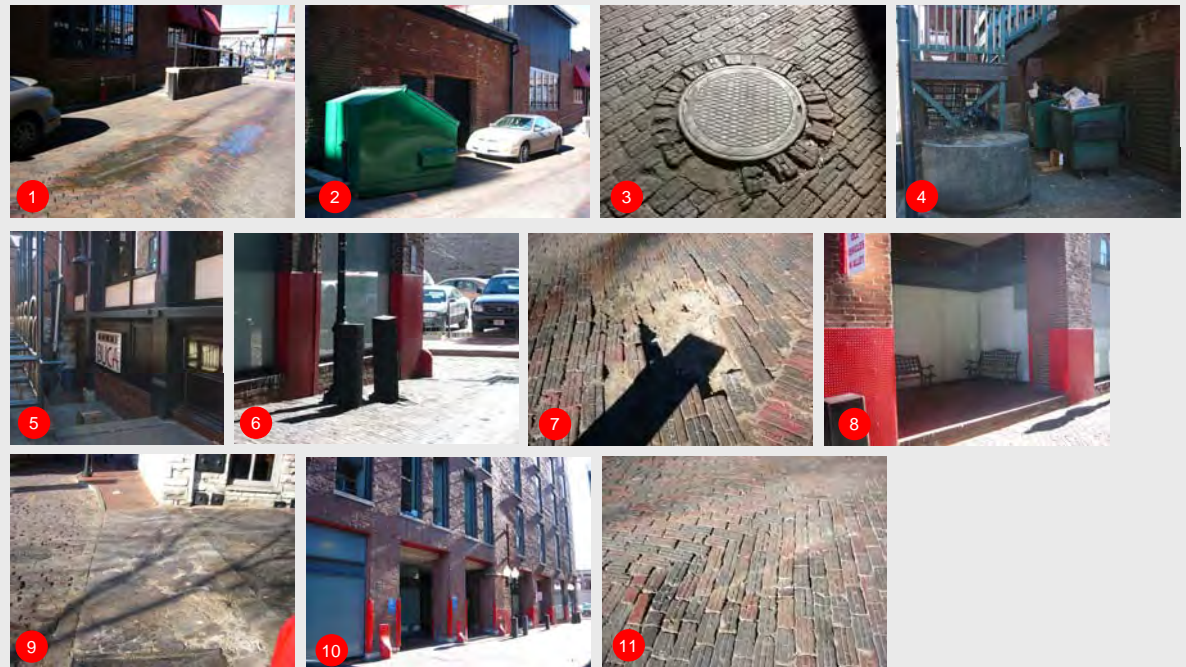
Key Map

### Legend

1. Poor drainage throughout alley. Alleys are designed to drain towards the center, but most water collects in the middle, but never gets out of the alley.
2. Unscreened dumpsters which should be located within the building envelope.
3. Pavement has settled or is missing adjacent to manholes creating tripping hazards.
4. Dumpster storage areas should be cleaned regularly.
5. Alley lacks orientation signage for businesses which have alley access.
6. Square bollards need to be straightened.
7. Paving bricks missing at various locations, and should be replaced.
8. Nice Informal seating area, however dumpsters make area unpleasant for users.
9. Steep transition to adjacent sidewalk. Alley drainage rarely exists center of alley. Alley needs to be properly pitched or permeable pavement should be utilized.
10. Additional landscape material should be incorporated onto alley.
11. Dark pavement adjacent to brick buildings contributes to urban heat island effect. Lighter colored pavements will reflect heat during summer months.



Site Map





# Visual Assessment

## Blocks 25 & 26 Morgan St.



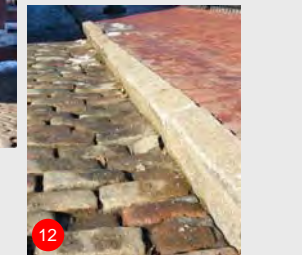
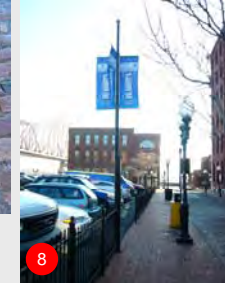
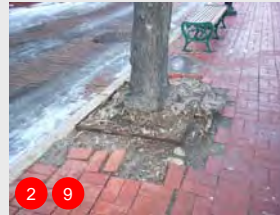
Key Map



Site Map

### Legend

1. Street trees needed on this side of street.
2. Trees heaved heaved surrounding pavement and tree grates. Several tree planting areas have missing or loose cobbles edges.
3. Newspaper box should be moved to a consolidated location with other boxes.
4. Various utility boxes need to be adjusted flush to grade.
5. Directory sign needs repainting and updated map.
6. Benches needed on this side of sidewalk.
7. Missing pavers in alley need to be replaced at various locations.
8. Banner pole adjacent to parking should include a flag pole for added visual interest at parking lot opening.
9. Missing pavers need to be replaced at various locations.
10. Various sign poles need to be repainted and straightened.
11. Traffic control gate should be painted black to match surrounding fixtures.
12. Curb needs replacement at various locations.



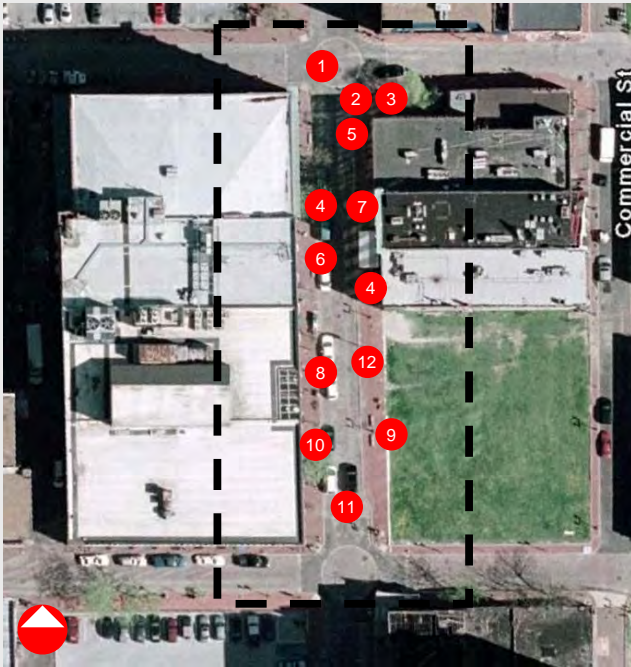


# Visual Assessment

## Blocks 26E & 15W First St.



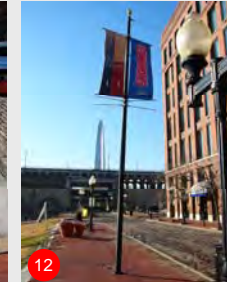
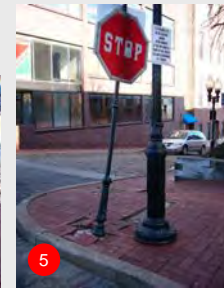
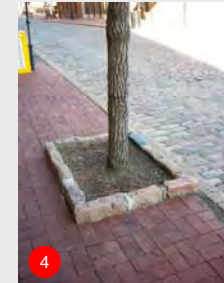
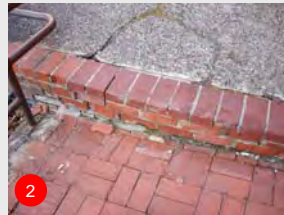
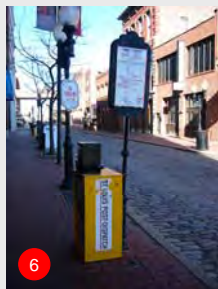
Key Map



Site Map

### Legend

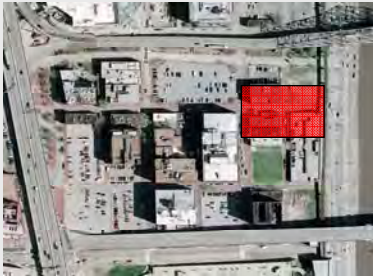
1. Intersection paving very uneven. Cobbles need to be reset and concrete banding should be replaced.
2. Brick steps should be rebuilt. Uneven pavement near steps needs to be reset.
3. Planting bed tie walls need replacement and the plantings need replacement.
4. New street trees are needed on both side of the street.
5. Paint and straighten sign poles.
6. Consolidate paper and delivery boxes into one location.
7. Additional planters would improve outdoor dining experience. Planters should match other site furnishings.
8. Multiple valve boxes need to be reset throughout area.
9. Existing benches need to be replaced.
10. Pedestrian pavement has settled and needs to be reset.
11. Handicap ramp needed on this side of street opposite existing ramp.
12. Existing banner poles should incorporate flags where possible.





# Visual Assessment

## Blocks 15 & 16 Morgan St.



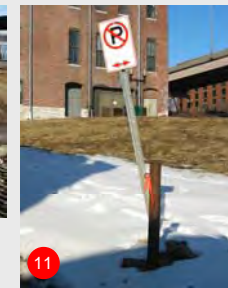
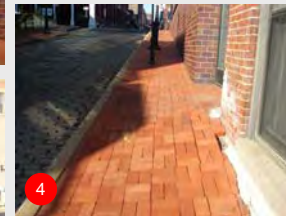
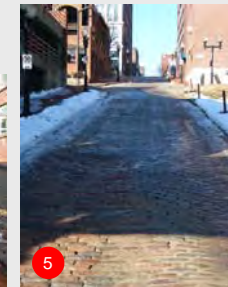
Key Map



Site Map

### Legend

1. Concrete band is major traffic barrier and should be replaced.
2. Missing light fixture needs to be replaced.
3. Sidewalk has width on this side of street to accommodate new street trees.
4. Major pavement warping at building door combined with light poles, create a barrier along sidewalk.
5. Sidewalk and street grade are too steep to accommodate ADA routes.
6. Pavement has settled around existing utility structures creating tripping hazards.
7. Asphalt overlays should be removed from street cobble stones and cobbles should be reset to create even driving surface.
8. Street trees could be added where sidewalk is wider on this side of street.
9. Existing granite curb.
10. No sidewalks exist on this side of street.
11. Non standard parking sign pole, should be replaced with consistent pole.
12. Cable and bollard barriers are ineffective and should be replaced with some other type of temporary barrier.
13. Silt collecting in this area due to erosion from adjacent vacant site.



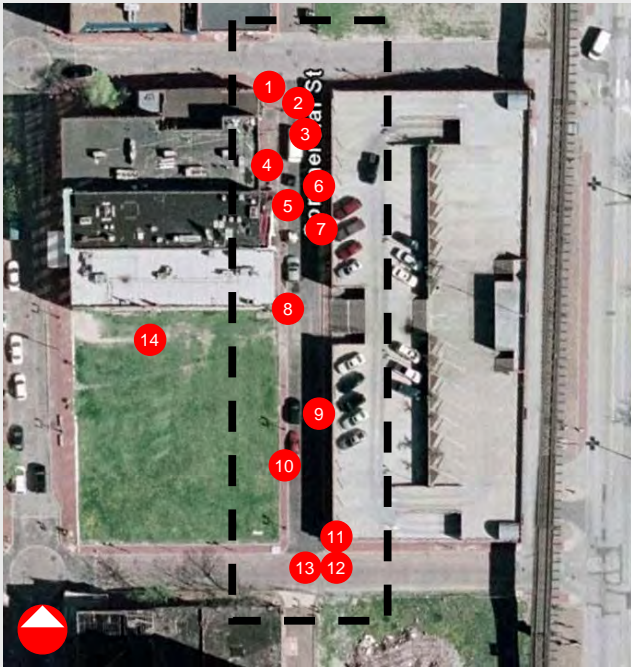


# Visual Assessment

## Blocks 15E & 15W Commercial Alley



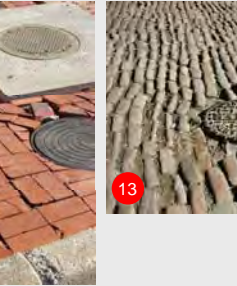
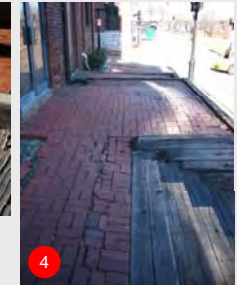
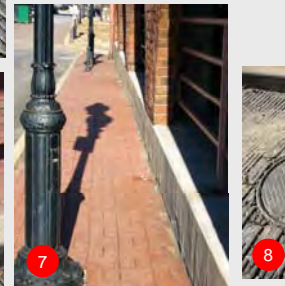
Key Map



Site Map

### Legend

1. Curbing and brick pavement need resetting in this area.
2. Alley brick needs resetting in this area.
3. Brick needs to be reset to provide positive drainage to adjacent inlet.
4. Entire rear service area needs to be reconstructed. Paving has settled. Retaining walls are at the end of their functional lifespan, and lack adequate safety rails.
5. Dumpsters need to be screened, or located within building envelopes/enclosures.
6. Sidewalk pavement has settled and needs to be reset throughout length of alley.
7. Sidewalks lack adequate width due to light poles located in the middle of sidewalks, and do not meet ADA guidelines.
8. Pavement settlement at existing manhole.
9. Permanent solution is needed for secondary parking garage access during high water events which require closure of the Leonor K. Sullivan Blvd. entrance.
10. Wooden retaining wall failing and in need of immediate replacement.
11. Major pavement/sewer replacement needed.
12. In-ground street sign replacement needed.
13. Lucas Street street cobbles need resetting.
14. Future redevelopment plans should include provision for pedestrian linkage to First Street.





# Visual Assessment

## Block 15E Leonor K. Sullivan Blvd.



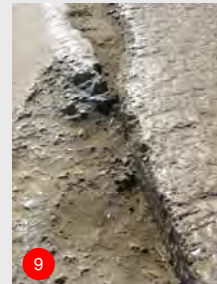
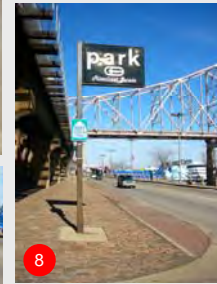
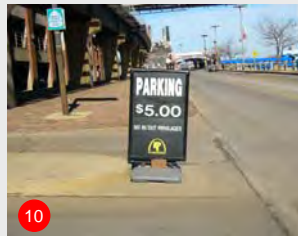
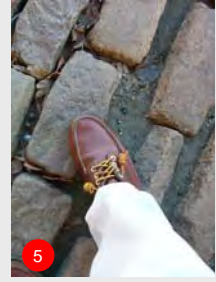
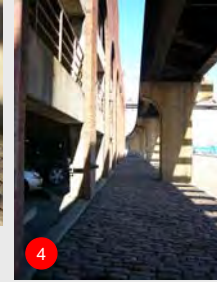
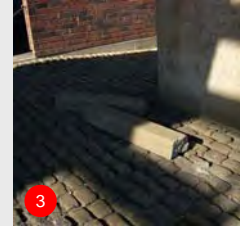
Key Map

### Legend

- Existing sign pole is bent, needs painting and should be replaced.
- Brick paving at fire hydrant needs to be reset.
- Existing damaged parking stops project a poor image.
- Consider adding pedestrian level lighting to garage piers to provide additional ambient lighting in this area.
- Cobblestone joints in pavement are too wide, making walking difficult and potentially unsafe. Cobbles should be reset.
- Missing cobblestone should be replaced at various locations.
- Brick sidewalk has settled and does not adequately drain. Mud is covering sidewalk at several locations.
- Parking garage sign needs painting. Consider a new sign designed to match character of Laclede's Landing marquee signs. A directional sign at this location would help improve visitor orientation.
- Remove existing curb and replace with roll type curb at parking garage entrance.
- Temporary parking price sign blocks ADA access. Consider incorporating price information to existing pole sign, described above.



Site Map





# Visual Assessment

## Block 66 Third St.



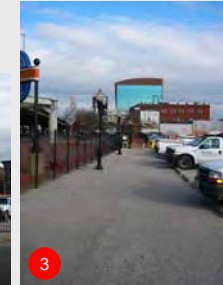
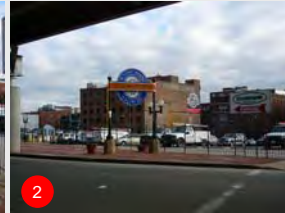
Key Map



Site Map

### Legend

1. Overpass pier intrudes into pedestrian space. Pier should be painted to compliment surrounding visual environment.
2. Area lacks tree planting, and planters are too small to match the scale of the space.
3. Space between fence and parking lot stalls is wasted and could be utilized better as a planting area. Area could benefit from a planted buffer.
4. Bridge equipment is visually unattractive and should be screened or treated to better blend into surrounding environment.
5. A higher impact sign is needed at this prominent location.
6. Pedestrian view terminates on an unattractive parking lot and related signage.
7. Eads Bridge/parking lot edge collects trash and creates a poor first impression of the Landing.
8. Area under I-70 overpass is unexciting and should be visually appealing to draw people into the Landing from Downtown.



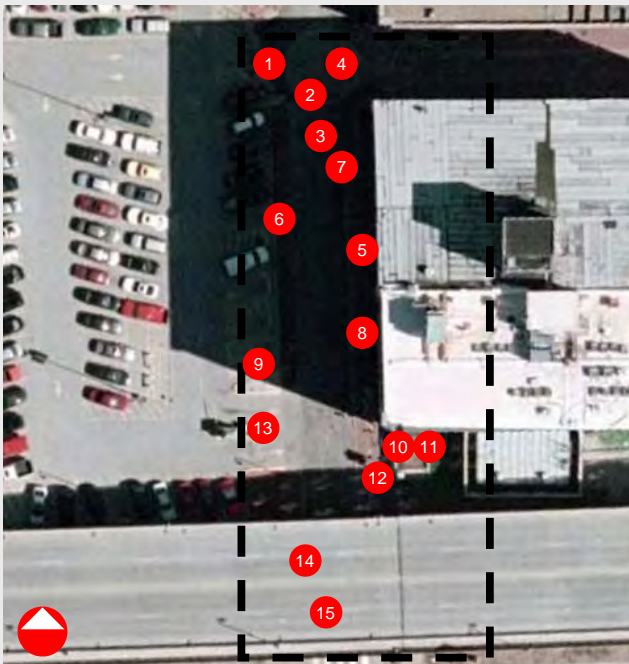


# Visual Assessment

## Block 66 & 27W Second St.



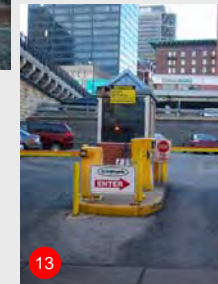
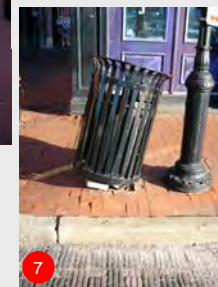
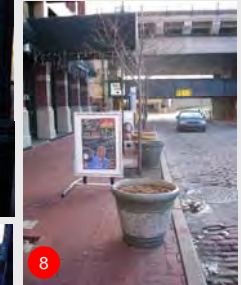
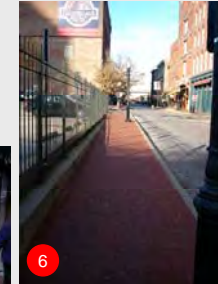
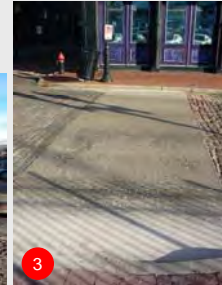
Key Map



Site Map

### Legend

1. A pedestrian oriented terminus is needed at the end of Lucas Street.
2. Circular concrete band needs replacement and uneven intersection pavers need to be reset.
3. Large concrete band with ADA ramp on one side but straight curb on opposite side.
4. Street pavement is rutted and in very bad condition. Needs cobble stone reset.
5. Benches in this area are missing and need to be replaced. Plant larger trees in a continuous bed to provide improved growing conditions. Replace missing tree grate or add plantings to open area.
6. Sidewalk on this side of street is narrow, but it needs planters to provide a softer edge.
7. Trash can needs to be reset.
8. Provide designated area to group sidewalk signs. Existing flag pole. Should be located in a more visible location possibly near newspaper boxes.
9. Eads Transit Park should be open to the public so daytime office users can access it.
10. Public art applied to existing wall. Difficult to see.
11. Paper boxes are grouped in this area with directory sign. Trash can should be provided in this location to control litter.
12. Signage at parking attendant booth should be consolidated to improve legibility.
13. Traffic control gate should be painted black to match color of surrounding fixtures.
14. Area beneath Eads Bridge is dark, and filthy. Overhead structure should be modified to prevent accumulation of pigeon droppings.



# Visual Assessment

## Blocks 27W & 27E Clamorgan Alley



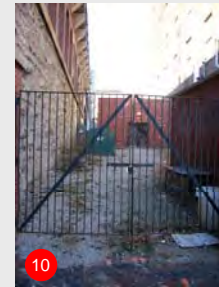
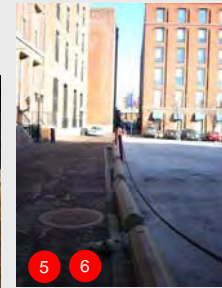
Key Map

### Legend

1. Poorly defined edge of parking lot. No buffer or screening.
2. Temporary wood stairs should be replaced with permanent brick solution.
3. Utilities are generally well screened in this alley.
4. Alley pavement has been patched with concrete and asphalt in various locations. Patches should be removed and replacement brick pavers installed.
5. Bollard and cable barrier projects poor impression. Replace with black metal fence similar to that on other edges of the adjacent parking lot.
6. Curb stops are unattractive and should be removed.
7. Storage barrels should be placed inside building envelope, or screened from view.
8. Dumpster is exposed and visually blocks views and pedestrian circulation through Eads Bridge portal.
9. Inlet is in poor condition, and pavement around storm inlet has settled preventing adequate drainage.
10. Poor view into utility service area. Needs to be screened.
11. Area beneath Eads Bridge is dark, and filthy. Overhead structure should be modified to prevent accumulation of pigeon droppings.
12. Consider adding attractive entry signage to Eads Bridge structure at portal entrance.



Site Map





# Visual Assessment

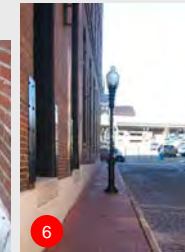
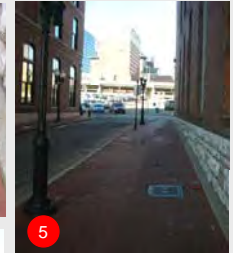
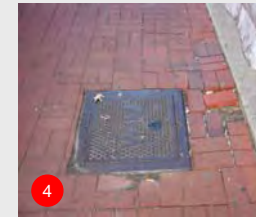
## Blocks 26 & 27 Lucas Ave.



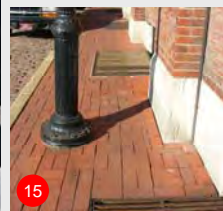
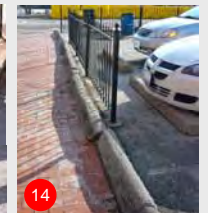
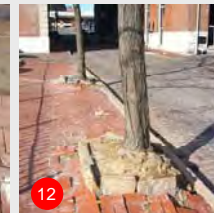
Key Map

### Legend

1. Cobble street pavement very uneven, resetting needed.
2. Fire hydrant blocks corner sidewalk access.
3. Valve boxes and horizontal street signs create multiple tripping hazards.
4. Sidewalk pavement around water meter needs to be reset.
5. Nice wide sidewalk, with lights located at the street edge of sidewalk.
6. Narrow sidewalks w/ lights in the center of sidewalk make wheelchair accessibility difficult.
7. Abandoned sign post should be removed or utilized.
8. Alley pavement surface in poor condition with steep cross slope.
9. Uneven pavement at all manholes is a serious tripping hazard.
10. Inlet grate is a tripping hazard and needs replacement.
11. Street light placement in center of sidewalk seriously compromises usefulness of sidewalk.
12. Mature trees have disrupted pavement. Trees should be replaced and pavement reset.
13. Benches need replacement in this section of sidewalk.
14. Parking lot edge constructed of asphalt which is crumbling. A new edge is needed.
15. Multiple grates combined with street light placement make walking difficult.
16. This sidewalk has additional width to accommodate trees and street lights.
17. Sidewalk constriction point due to narrow sidewalk and light pole location.



Site Map



# Visual Assessment

## Block 27E & 14W First St.



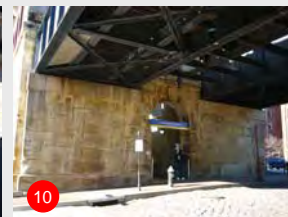
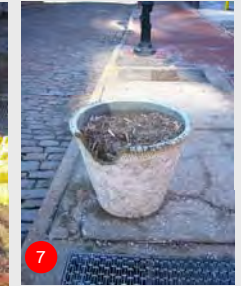
Key Map

### Legend

1. Asphalt curb along parking lot fence should be replaced. Fence needs to be removed and reset. Street trees completely missing and should be replaced in this section for First Street.
2. Curbing needs replacement and street pavement needs to be reset.
3. Sidewalk in front of Switzer Building site is gone and needs to be replaced. Due to building removal, a barrier fence will also be needed.
4. Sidewalk very uneven due to pavement settling and parking lot entry. Cross slope at parking lot entry is too steep to be ADA compliant.
5. Small pedestrian pavement settlement area will require reconstruction of pavement sub base.
6. Damaged planters should be replaced. Replacement planters should match surrounding architectural style
7. Temporary parking signs located on sidewalk need to be removed from sidewalk.
8. Roadway pavement under Eads Bridge is in very bad shape and needs to be reset.
9. Area beneath Eads Bridge is filthy. Overhead structure should be modified to prevent accumulation of pigeon droppings.
10. Consider adding attractive entry signage to Eads Bridge structure at portal entrance.



Site Map





# Visual Assessment

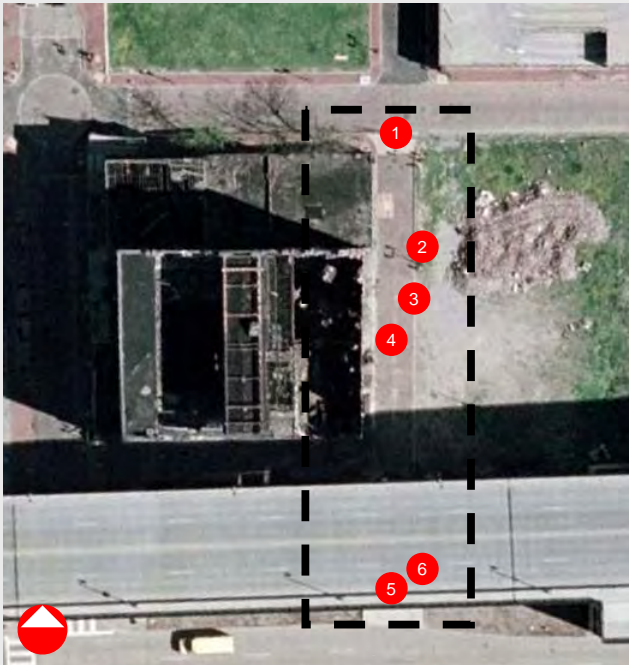
## Blocks 14W & 14E Commercial Alley



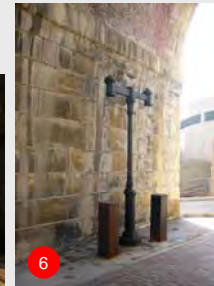
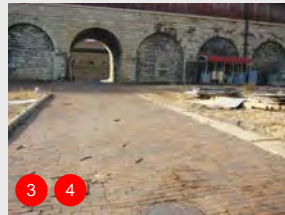
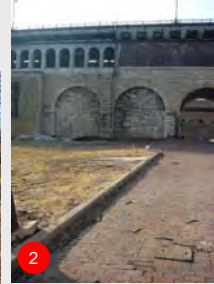
Key Map

### Legend

1. Access to alley is completely restricted due to a construction fenced placed around the Switzer Building site.
2. All pedestrian lighting has been removed from this alley during the demolition of the Switzer Building. Lighting which matches the historic lighting used throughout the Landing should be used as replacement lighting.
3. Only a small portion of the existing alley curbing remains. New curbing should be installed when property is redeveloped, or when construction fence is removed.
4. Alley pavement is generally in good shape and does not need replacement.
5. Eads Bridge portal is clean but needs graffiti removal and painting. Consider adding attractive entry signage at portal entrance.
6. Replace missing lighting fixtures to provide adequate lighting in portal area.



Site Map



# Visual Assessment

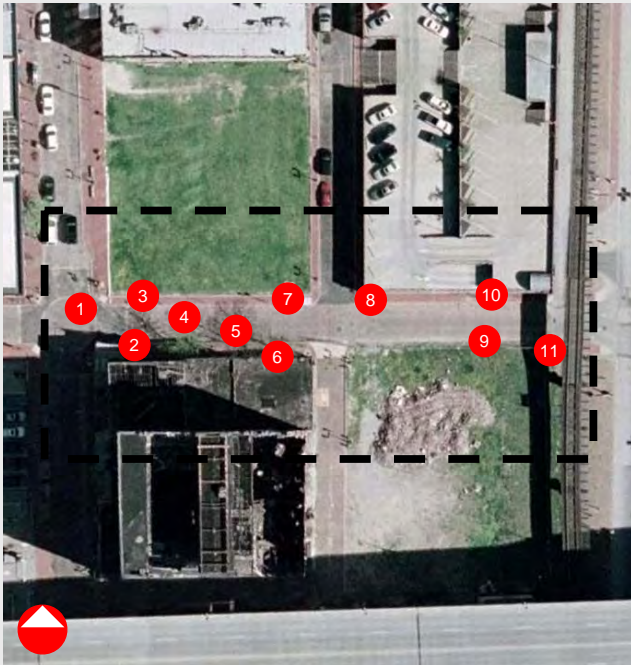
## Blocks 14 & 15 Lucas Ave.



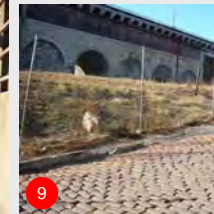
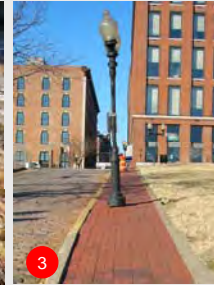
Key Map

### Legend

1. Cobble street pavement very uneven especially at manholes.
2. Existing iron columns embedded in building foundation wall are a hazard and should be removed. A safety barrier rail is needed along entire length of remaining foundation wall.
3. Sidewalk lacks adequate width due to light poles located in the middle of sidewalk, and does not meet ADA width guidelines. Light fixtures need to be straightened.
4. This section of Lucas Ave. has a very steep slope. ADA slope guidelines can not be met on this sidewalk.
5. Mature trees have disrupted pavement surface. Trees should be replaced and pavement repaired.
6. Remaining building foundation wall will need remedial work to make it safe for pedestrians.
7. Existing inlet needs to be lowered to meet surrounding pavement grade.
8. Garage downspout and adjacent light pole make walkway very narrow and difficult for pedestrians.
9. Sidewalks and curb do not exist in this area.
10. Sidewalk adjacent to garage lacks adequate width due to light poles located in the middle of sidewalk, and does not meet ADA width guidelines.
11. Electric boxes need to be reset flush to surrounding pavement to eliminate tripping hazard.



Site Map





# Visual Assessment

## Block 14E Leonor K. Sullivan Blvd.



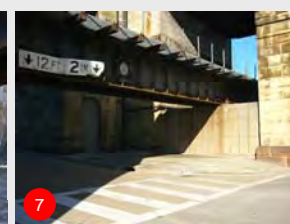
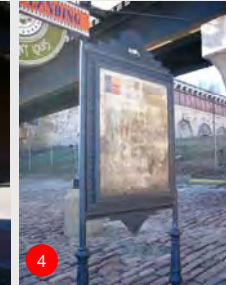
Key Map

### Legend

1. Pavement adjacent to traffic signal boxes has settled and needs to be reset. Inlet also needs new lid.
2. Small portion of cobble area needs to be reset.
3. Street sign needs to be realigned to proper streets.
4. Directory needs to be cleaned of mud so sign is legible.
5. Mud from adjacent property is covering cobble stones. Erosion control from adjacent property should be installed and maintained.
6. Public art is in a low visibility area, and should be relocated to a higher visibility location.
7. Highly visible, but neglected area should include signage and planters to serve as a point of welcome to the Landing.

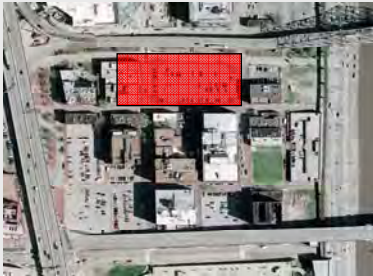


Site Map



# Visual Assessment

## Block 25 Parking Lot



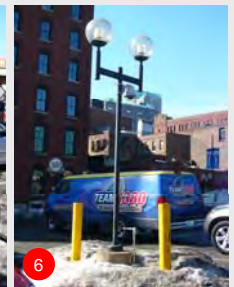
Key Map

### Legend

1. Pedestrian access opening marked by taller posts. Banners would add visual interest.
2. Secondary supports indicate fence will require remounting in the future.
3. Scattered curb stops used to define edge of lot. Trash cans do not match style of others used throughout the Landing.
4. Supply storage box should be screened.
5. Electric meter should be screened from view.
6. Parking lot lights do not match landing style of lights used in landing.
7. Attendant booth should reflect some elements of architectural style of area.
8. Planting island are completely lacking in this parking lot.



Site Map





# Visual Assessment

## Block 66 Parking Lot



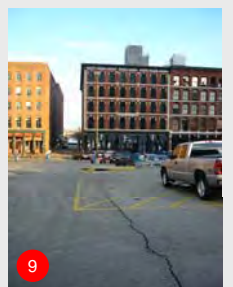
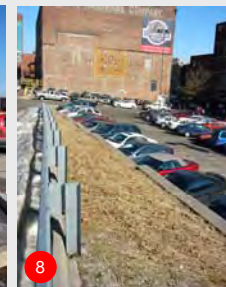
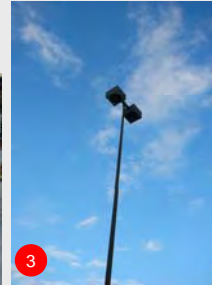
Key Map



Site Map

### Legend

1. Planting island are completely lacking in this parking lot. Planting area could be created in this area without any loss of parking spaces.
2. ADA spaces have access to third Street only at Main entrance. Creating a pedestrian opening closer to these spaces would improve access.
3. High mast light fixture is not in character with other light fixtures used throughout the Landing.
4. Access to alley should be kept free of parking.
5. Attendant booth should reflect some elements of architectural style of area. Signage should be consolidated for improved legibility.
6. Secondary booth should be removed if not used.
7. Excess space should be converted into planting area to provide a visual buffer to the parking lot.
8. Planting should be added on upper level of retaining wall end island.
9. Consider installation of dedicated pedestrian access route through parking lot to downtown.



# Visual Assessment

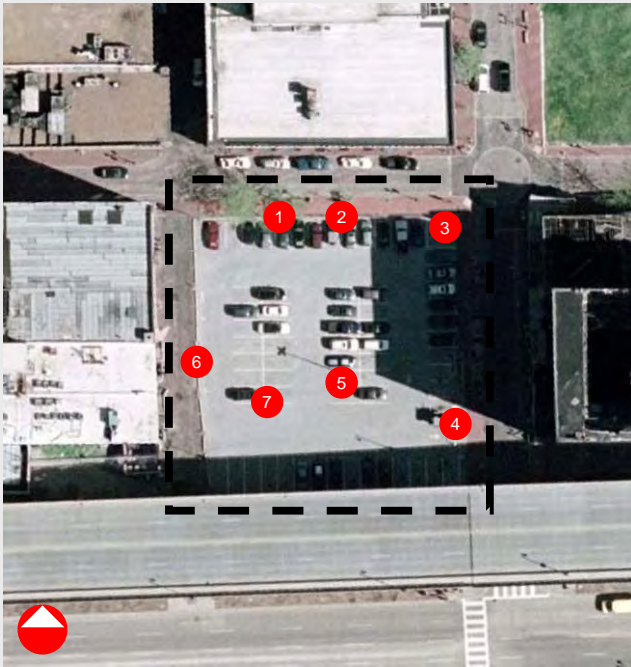
## Block 27E Parking Lot



Key Map

### Legend

1. Pedestrian opening hard to see due to no upright elements are used to mark the opening.
2. Asphalt curb edge is disintegrating and needs to be replaced
3. Potential corner for planting. Since storm drain is in this area, bio treatment could be utilized.
4. Attendant booth should reflect some elements of architectural style of area. Signage should be consolidated for improved legibility. Lighting at booth is a major source of glare.
5. High mast light fixture is not in character with other light fixtures used throughout the Landing.
6. Cable barrier prevents pedestrian access from this side of lot. Barrier also presents poor first impression of the Landing.
7. Planting islands should be incorporated into end islands of lot.



Site Map





# Visual Assessment

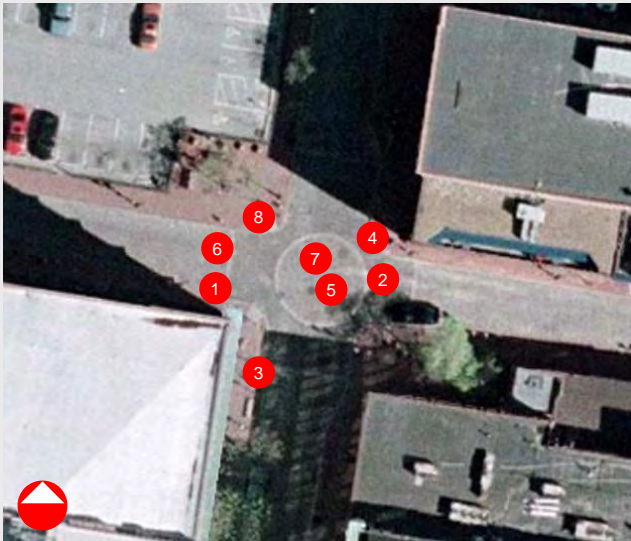
## Intersection North First & Morgan Street



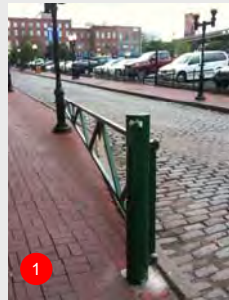
Key Map

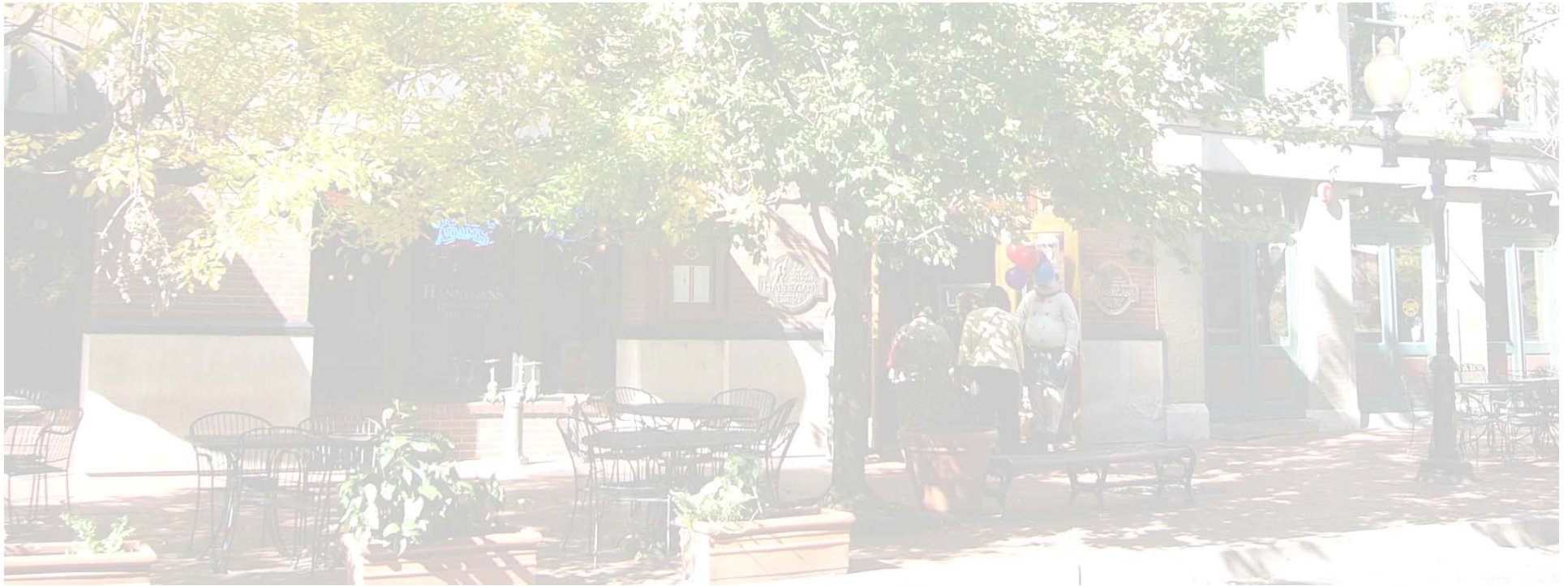
### Legend

1. Existing swing gate should be painted black to match surrounding fixtures.
2. Paving bricks around existing manhole need to be reset and flush with concrete band and manhole. Circular concrete band needs replacement.
3. Existing planter should include tree for shade and to match surrounding tree planters.
4. Paving bricks need to be reset and flush with concrete band for accessibility. Sand and debris needs to be cleaned and maintained.
5. Cobblestone joints in pavement are too wide, making walking difficult and potentially unsafe. Cobbles should be reset.
6. Cobblestones around manhole need to be reset and flush with manhole. Concrete band needs replacement.
7. Adjacent fountain equipment housing needs to be repaired, replaced and/or relocated.
8. Existing plaza space a good refuge for pedestrians and source of shade. Enhance with additional plantings in tree planter.



Site Map





# Plan Development



# Plan Development

## Plan Development

The Plan Development (**Tab #4**) contains the priority elements established during the planning process. This section provides recommendations on the most compelling elements that require the highest level of attention. Factors for establishing the priorities include (1) affordable aesthetic enhancements; (2) pedestrian safety and comfort; (3) significant projects that when implemented will make a distinctive difference to the visual environment; (4) selecting “doable” projects for demonstration of what can be accomplished within the fiscal resources of the District.

The Plan Development section contains details of the Washington Street Beautification Project, along Washington from Memorial Drive to Leonor K. Sullivan Blvd.

The Plan Development contains guidance on the following elements:

- Improvement Priorities
  - Working Committee
  - Areas
  - Street Tree Planting Zones
  - Special Projects
    - Portal - First St. at Washington St.
    - Lucas Pedestrian Extension through parking lot
    - Building Sign Painting
    - Bridge Portal Sign
- Design Guidelines
  - Elements
  - Sidewalks, Curbs & Edges
  - Core Intersections
  - Street Tree Palette
  - Tree Boxes
  - Benches
  - Planters
  - Bollards
  - Trash Receptacles
- Washington Street Beautification Plan

## Improvement Priorities

### Working Committee

Priority Comments from Working Committee  
February 4, 2009

1. The most bang for the buck.
2. Washington Avenue – done!
3. Lucas street – access through parking
4. Fence along Switzer property along 1<sup>st</sup> street
5. Heavy duty stuff first
6. West entrance into the Landing, add Las Vegas type sign “Laclede’s Landing”
7. Signage on Eads Bridge
8. Top of the Hill (Washington at Eads Bridge) sign
9. Do the street
10. Entrances are gross, tentacles out to other area
11. Plants to dress up the streetscape.
12. Entrances
13. Benches
14. Street lighting
15. Area Eyesore
16. Planters - great





# Improvement Priorities

## Areas

- On-going Sidewalk and Curb Replacement & Repair.
- Street Tree Planting Zones.
- Replace Site Furnishings:
  - Core Area
  - Pedestrian Plazas
- Pedestrian Plazas:
  - Clock Plaza
  - 1<sup>st</sup> Street at Morgan
- The Entry Portal At:
  - 3<sup>rd</sup> Street at Laclede's Landing Blvd. (LLB)
  - The Lumiere Entry at 2<sup>nd</sup> Street
- The Eads Bridge Entry Portals At Washington St and:
  - First Street
  - Second Street
  - Clamorgan Alley
- Street Tree Plantings Along North Side of LLB at Park.
- Pedestrian Access Through Parking Lot At Lucas Between 2nd and 3<sup>rd</sup> St.
- Parking Lot Edge Conditions
- Portal Signs At
  - Washington and 3<sup>rd</sup>
  - Washington and Leonor K. Sullivan Blvd. (LKSB)
  - LLB and LKSB
  - West Side of I-70 Overpass at LLB
- Building Sign Painting
- Rebuild Core Intersections
- Edge Condition Along Vacant Lots at Back of Sidewalks



Street Tree Planting Zones



Pedestrian Plazas



Entry Portals



Pedestrian Access Through Parking Lot



Portal Signs



Core Intersections



## Improvement Priorities

### Street Tree Planting Zones

- Street Tree Planting Zones
  - Typical - standard sidewalk (10')
  - Typical – wide sidewalk
  - Typical – narrow sidewalk
- Remove trees from the center of narrow walkways. Replant trees at the back side of narrow walkways, may require planting easements.
- On standard sidewalks and wide sidewalks consider using rectangular tree boxes.
- On standard and wide sidewalks reestablish tree boxes and replant trees at original planted locations.
- At Third Street Plaza: Save existing trees, remove remnants of metal tree guards, prune and shape existing trees to remain, enlarge tree boxes and remove and replace deformed/dead trees.



Street Tree Planting Zones



Pedestrian Plazas



Entry Portals



Pedestrian Access Through Parking Lot



Portal Signs



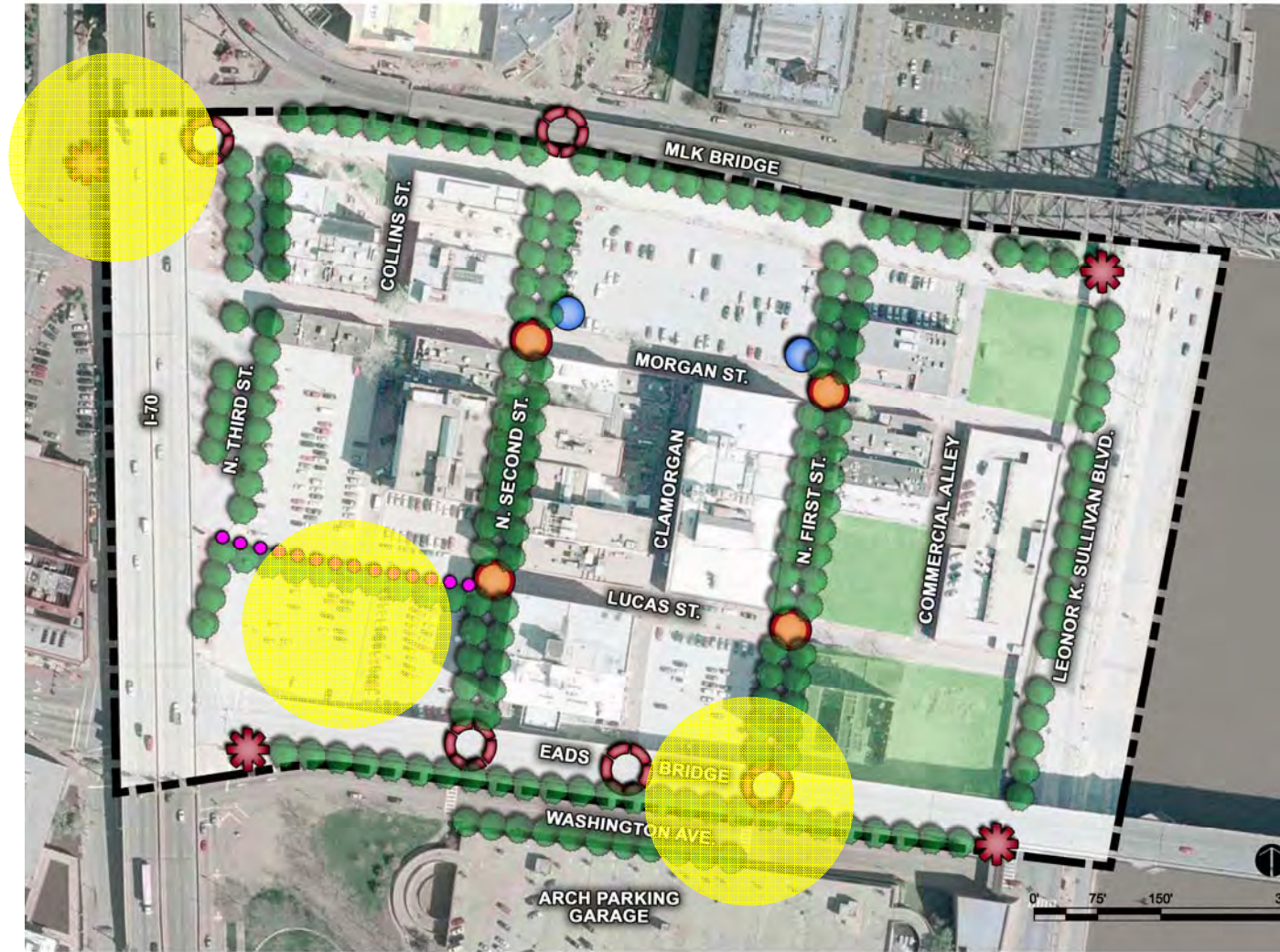
Core Intersections



# Improvement Priorities

## Special Projects

- Portal at First Street & Washington
- Lucas Extension Through Parking Lot
- Building Sign Painting
- Bridge Portal Sign



Street Tree Planting Zones



Pedestrian Access Through Parking Lot



Pedestrian Plazas



Portal Signs



Entry Portals



Core Intersections



## Special Projects

### Portal First Impressions

- Consider “pigeon dropping screen” above walkway on west side of First Street.
- Remove unused light post.
- Clean and reset sidewalk pavers.
- Add street trees to west side of First Street leading up to the bridge structure.
- Add new planters at entrance of bridge structure.





Special Projects

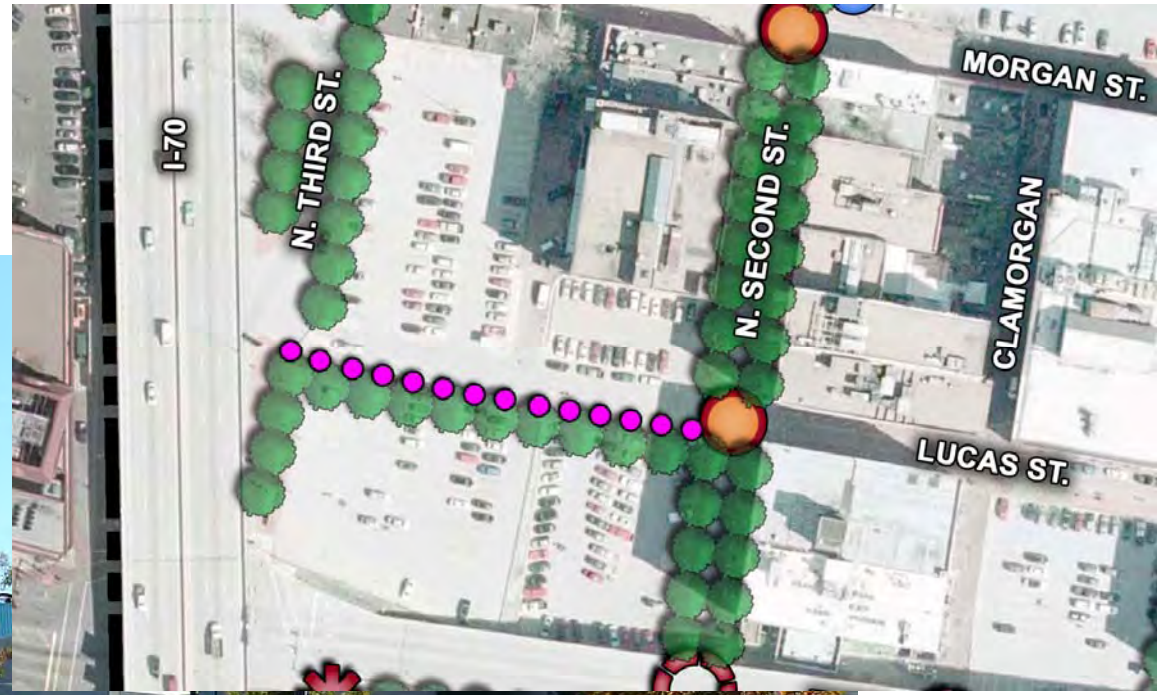
**Lucas Connector** Before





# Special Projects

## Lucas Connector After





Special Projects

**Building Sign Painting**





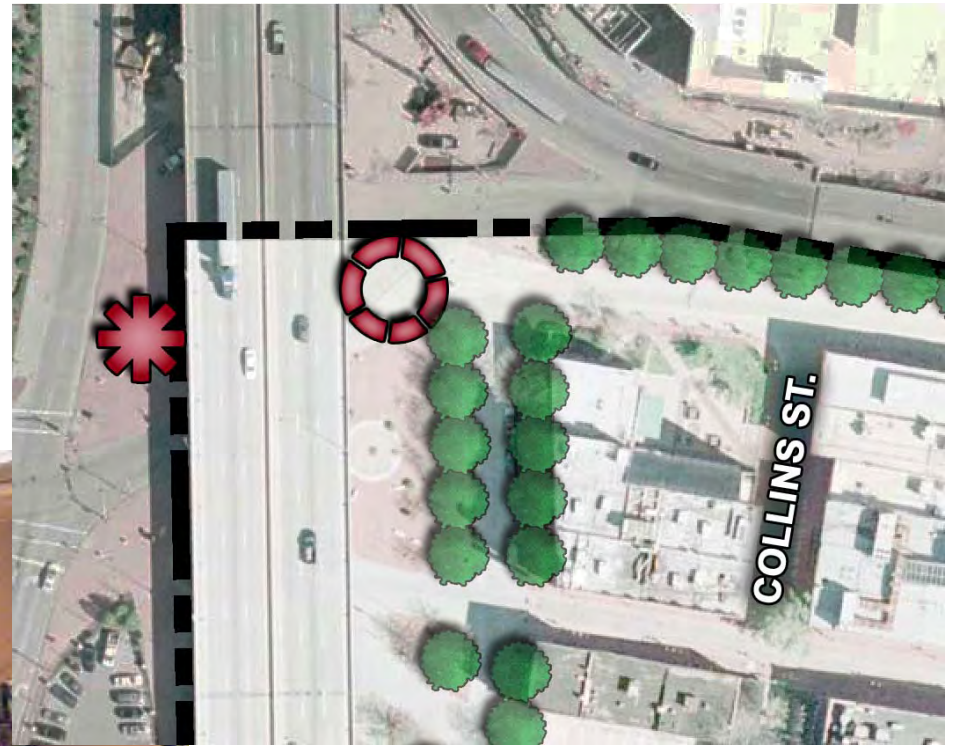
Special Projects

**Bridge Portal Sign**





Special Projects  
**Bridge Portal Sign**





# Design Guidelines

## Elements

Focus improvements on the elements which make up the public realm and systematically improve.

- Sidewalks, Curbs & Edges
- Core Intersections
- Street Tree Palette
- Tree Boxes
- Benches
- Planters
- Bollards
- Trash Receptacles



Street Tree Planting Zones



Pedestrian Plazas



Entry Portals



Pedestrian Access Through Parking Lot



Portal Signs



Core Intersections



## Design Guidelines

### Sidewalk, Curbs and Edges

Continue the systematic resetting of brick sidewalks, concentrating on the core area of the district first.



Street Tree Planting Zones



Pedestrian Plazas



Entry Portals



Pedestrian Access Through Parking Lot



Portal Signs



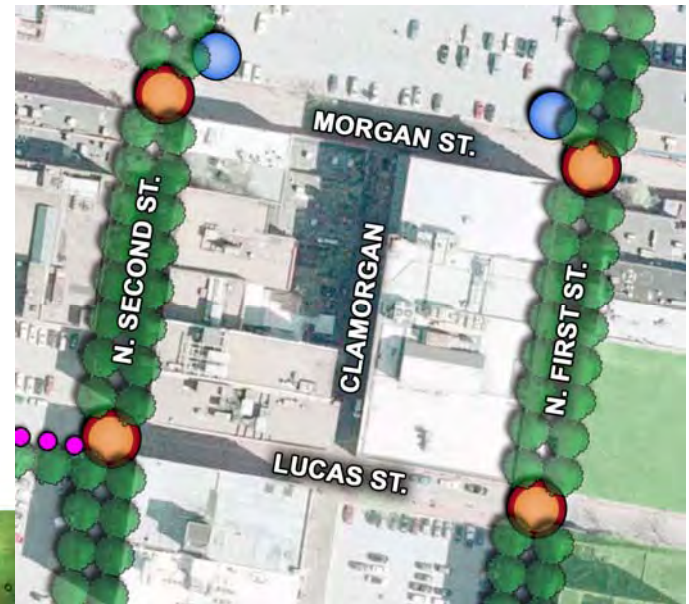
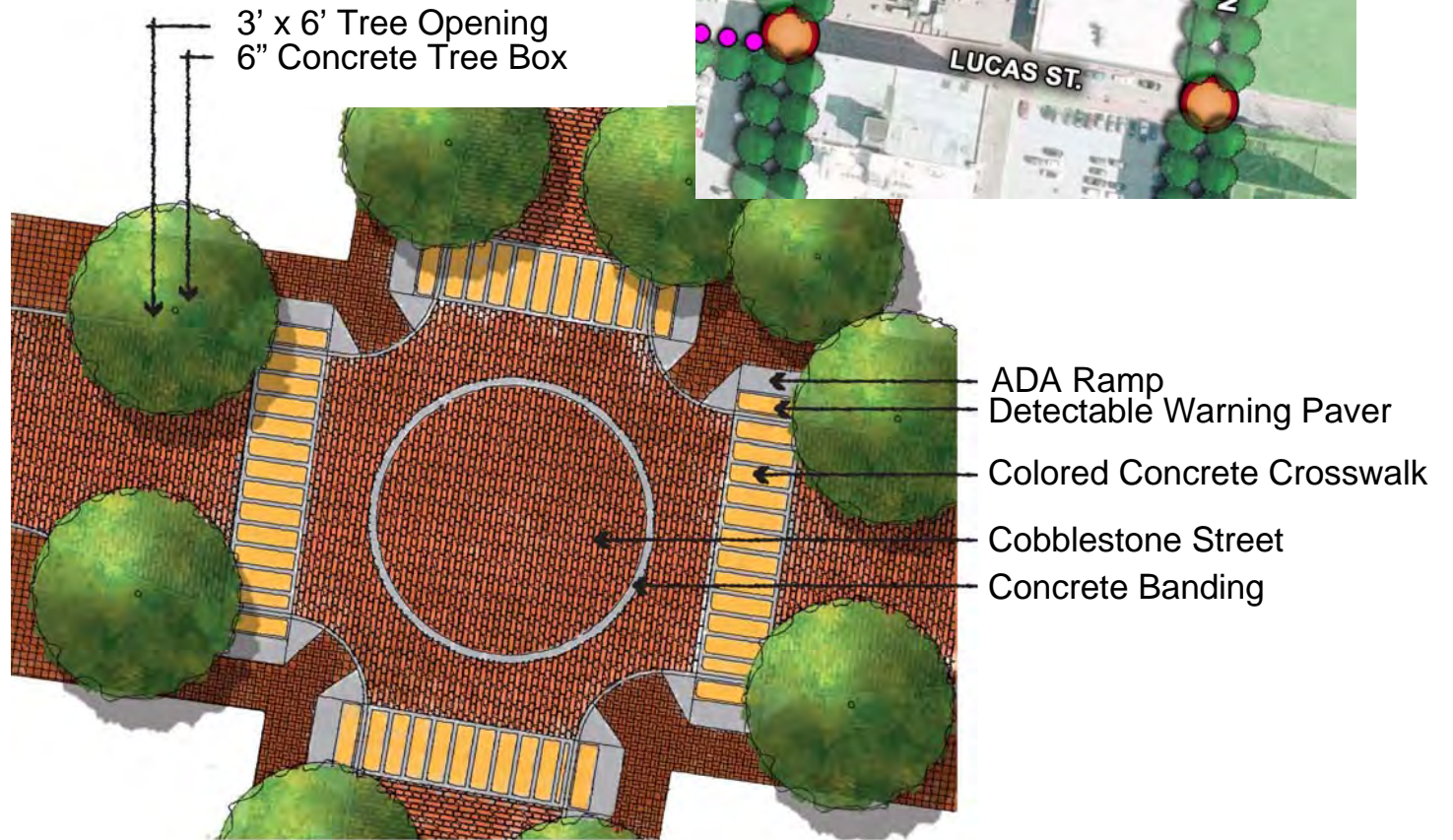
Core Intersections



# Design Guidelines

## Core Intersections

Renovate Core Intersections to reflect original paving design while improving the Handicapped Accessible routes through the District.





# Design Guidelines

## Street Trees Columnar Trees



Armstrong II Maple



English Oak



Columnar Linden

### Plant Palette

BOTANICAL NAME	COMMON NAME	SIZE	CONDITION
<b>Columnar Trees</b>			
Acer rubrum x freemanii 'Armstrong Two'	Armstrong II Maple	2"	B&B
Quercus robur 'Skyrocket' or 'Fastigiata'	English Oak	2 1/2"	B&B
Tilia cordata 'Swedish Upright' or 'Erecta' or 'Bohlje'	Columnar Linden	2"	B&B
<b>Canopy Trees</b>			
Gleditsia triacanthos var. inermis 'Skyline'	Skyline Thornless Honeylocust	2 1/2"	B&B
Platanus acerifolia 'Bloodgood'	Bloodgood' London Planetree	2 1/2"	B&B
Ulmus carpinifolia 'Pioneer' or 'Homestead'	Pioneer or Homestead Smoothleaf Elm	2 1/2"	B&B
Zelkova serrata 'Green Vase'	Zelkova	2 1/2"	B&B
<b>Ornamental Trees</b>			
Acer tataricum subsq. Ginnala	Amur Maple	2"	B&B
Amelanchier arborea	Serviceberry	2" (M/S)	B&B
Cercis canadensis	Eastern Red Bud	2"	B&B
Koelreutaria paniculata	Panicled Golden Raintree	2"	B&B

# Design Guidelines

## Street Trees Canopy Trees



Skyline Thornless Honeylocust



Bloodgood London Planetree



Homestead Smoothleaf Elm



Zelkova

### Plant Palette

BOTANICAL NAME	COMMON NAME	SIZE	CONDITION
<b>Columnar Trees</b>			
<i>Acer rubrum</i> x <i>freemanii</i> 'Armstrong Two'	Armstrong II Maple	2"	B&B
<i>Quercus robur</i> 'Skyrocket' or 'Fastigiata'	English Oak	2 1/2"	B&B
<i>Tilia cordata</i> 'Swedish Upright' or 'Erecta' or 'Bohlje'	Columnar Linden	2"	B&B
<b>Canopy Trees</b>			
<i>Gleditsia triacanthos</i> var. <i>inermis</i> 'Skyline'	Skyline Thornless Honeylocust	2 1/2"	B&B
<i>Platanus acerifolia</i> 'Bloodgood'	Bloodgood' London Planetree	2 1/2"	B&B
<i>Ulmus carpinifolia</i> 'Pioneer' or 'Homestead'	Pioneer or Homestead Smoothleaf Elm	2 1/2"	B&B
<i>Zelkova serrata</i> 'Green Vase'	Zelkova	2 1/2"	B&B
<b>Ornamental Trees</b>			
<i>Acer tataricum</i> subsp. <i>Ginnala</i>	Amur Maple	2"	B&B
<i>Amelanchier arborea</i>	Serviceberry	2" (M/S)	B&B
<i>Cercis canadensis</i>	Eastern Red Bud	2"	B&B
<i>Koelreuteria paniculata</i>	Panicked Golden Raintree	2"	B&B



# Design Guidelines

## Street Trees Ornamental Trees



Acer tataricum subsq. Ginnala



Redbud



Serviceberry



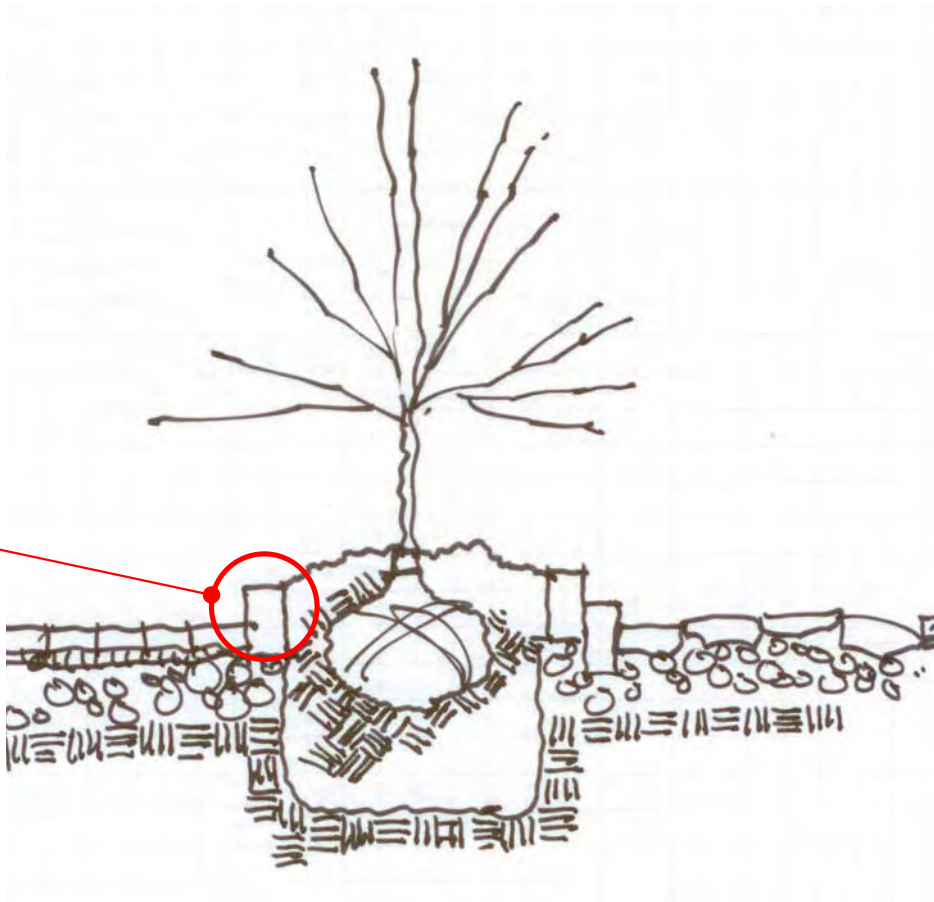
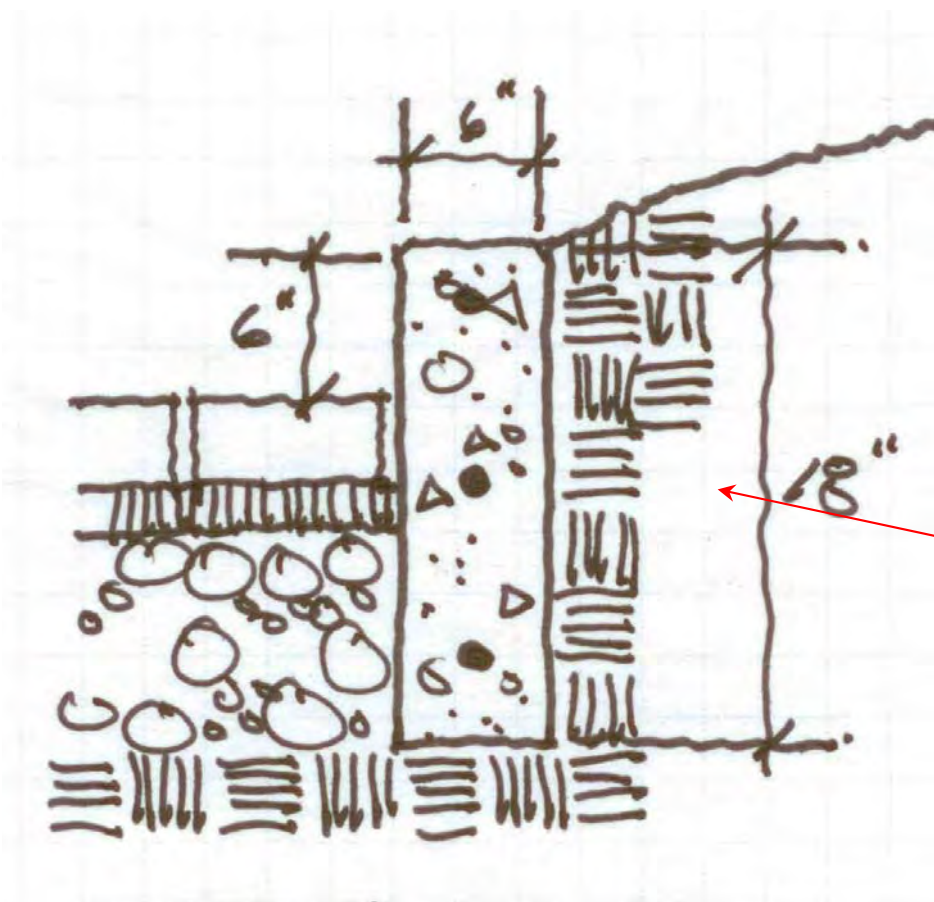
Panicked Golden Raintree

### Plant Palette

BOTANICAL NAME	COMMON NAME	SIZE	CONDITION
<b>Columnar Trees</b>			
Acer rubrum x freemanii 'Armstrong Two'	Armstrong II Maple	2"	B&B
Quercus robur 'Skyrocket' or 'Fastigiata'	English Oak	2 1/2"	B&B
Tilia cordata 'Swedish Upright' or 'Erecta' or 'Bohlje'	Columnar Linden	2"	B&B
<b>Canopy Trees</b>			
Gleditsia triacanthos var. inermis 'Skyline'	Skyline Thornless Honeylocust	2 1/2"	B&B
Platanus acerifolia 'Bloodgood'	Bloodgood' London Planetree	2 1/2"	B&B
Ulmus carpinifolia 'Pioneer' or 'Homestead'	Pioneer or Homestead Smoothleaf Elm	2 1/2"	B&B
Zelkova serrata 'Green Vase'	Zelkova	2 1/2"	B&B
<b>Ornamental Trees</b>			
Acer tataricum subsq. Ginnala	Amur Maple	2"	B&B
Amelanchier arborea	Serviceberry	2" (M/S)	B&B
Cercis canadensis	Eastern Red Bud	2"	B&B
Koelreutaria paniculata	Panicked Golden Raintree	2"	B&B

# Design Guidelines

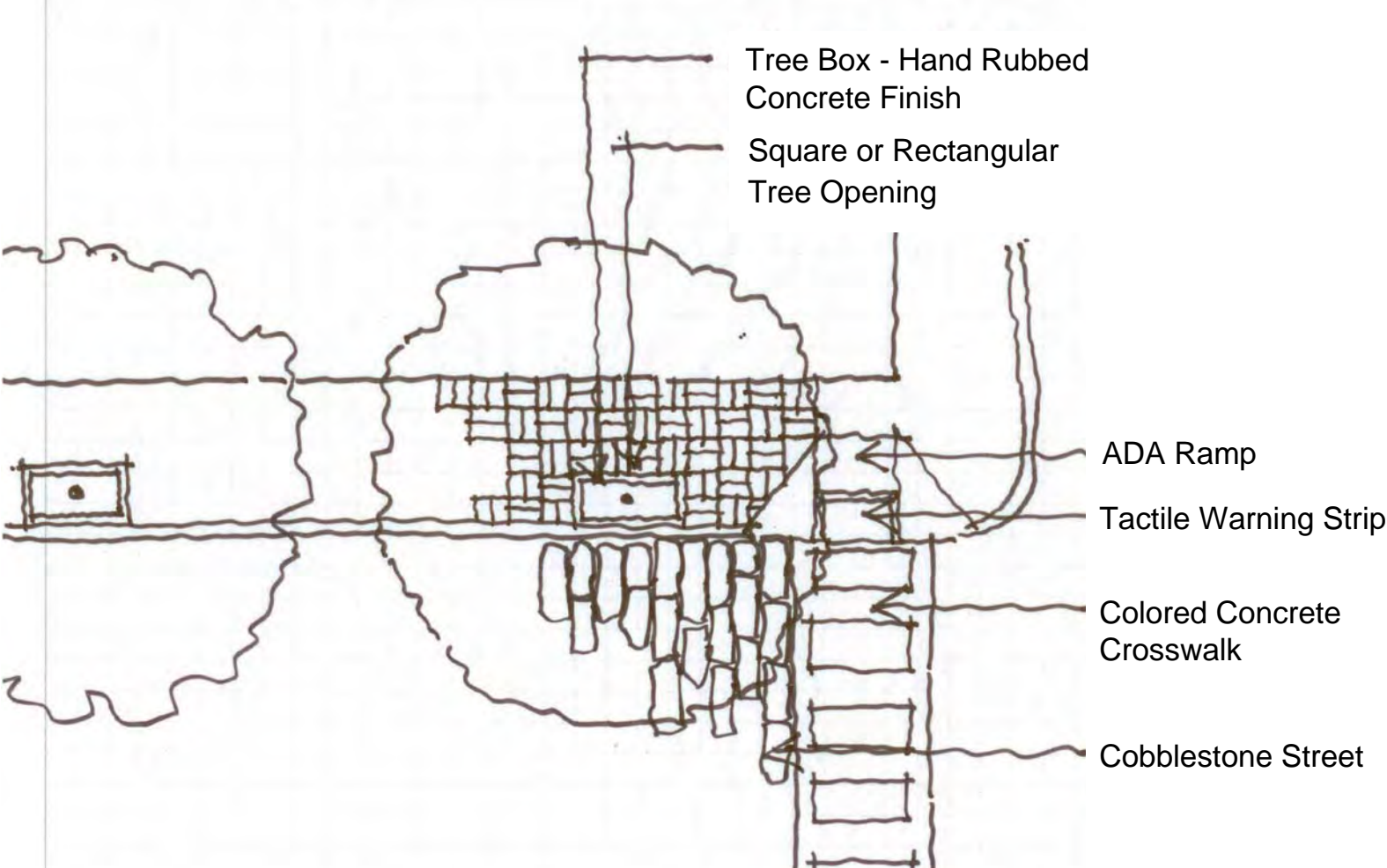
## Tree Boxes Concrete





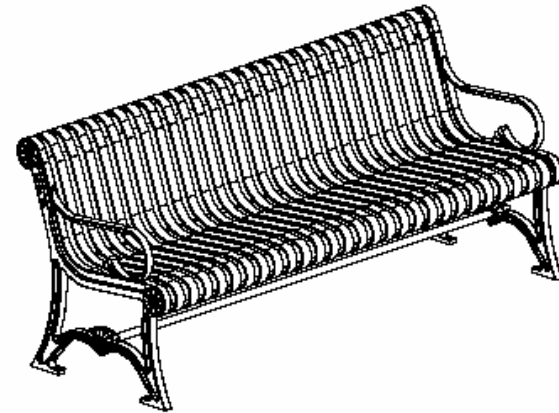
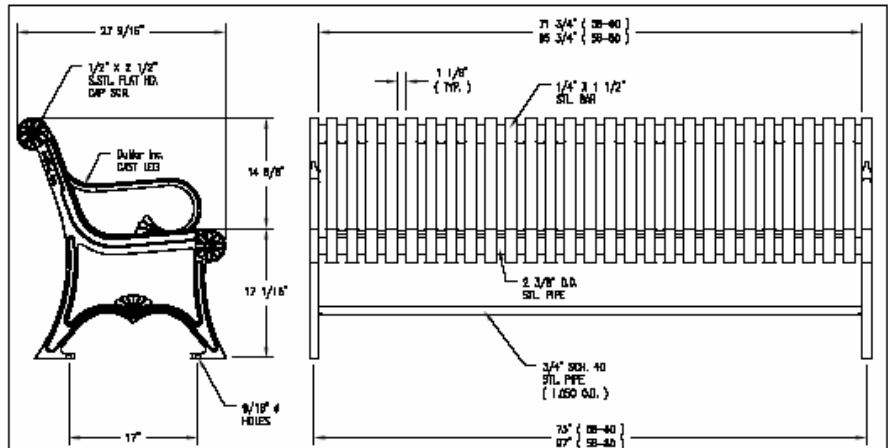
# Design Guidelines

## Tree Boxes Concrete



# Design Guidelines

## Bench With Back



### NOTES

- 1.) ALL STL. MEMBERS COATED W/ ZINC RICH EPOXY THEN FINISHED W/ POLYESTER POWDER COATING.
  - 2.) 1/2" x 3 3/4" EXPANSION ANCHOR BOLTS PROVIDED.
  - 3.) CUSTOM LETTERING AVAILABLE FOR RECESSED SIDE PANELS ( TOTAL OF 37 SPACES )
- CUSTOM LETTERING ( 37 SPACES )

### LENGTH OPTIONS

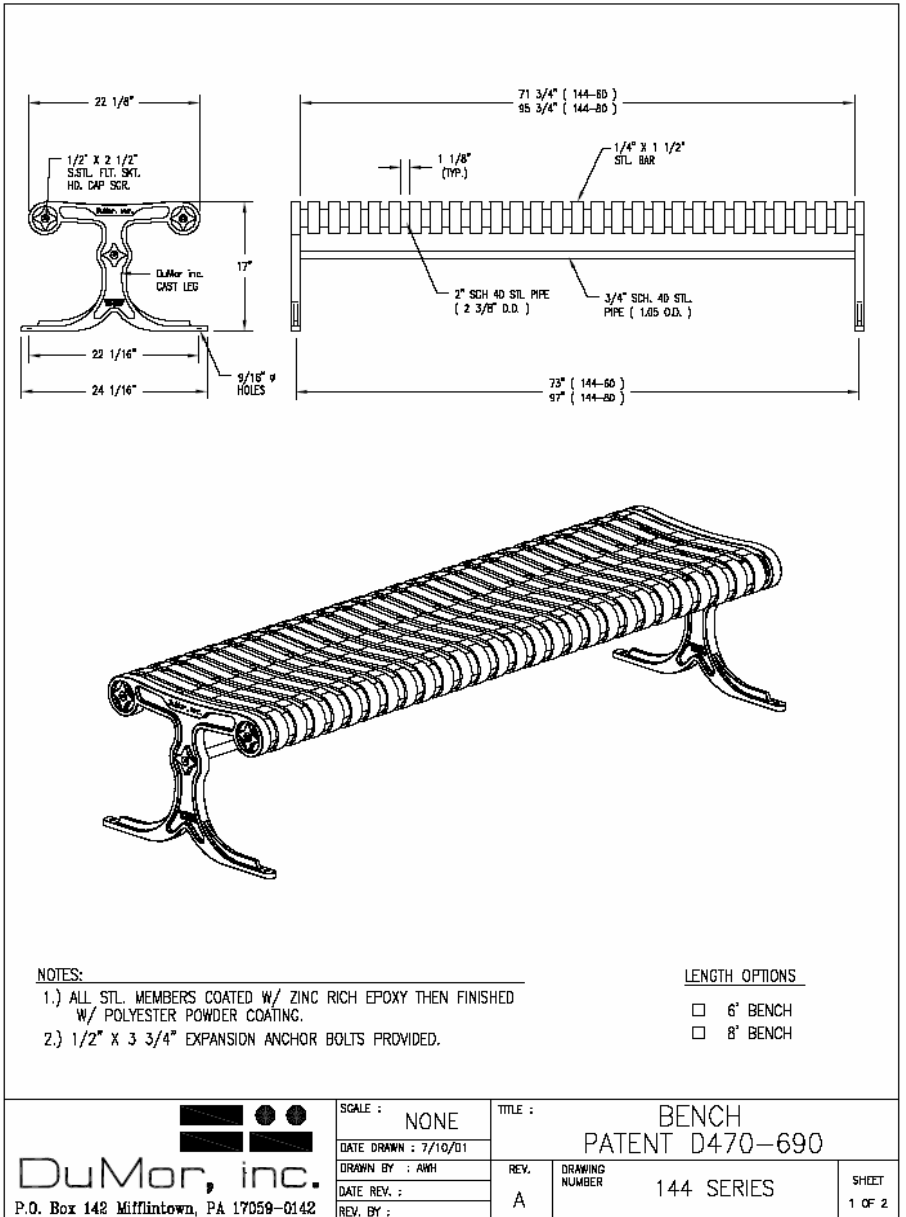
- 6' BENCH  
 8' BENCH

		SCALE : NONE	TITLE : BENCH	
DATE DRAWN : 3/22/94		REV. D	DRAWING NUMBER 58 SERIES	SHEET 1 OF 2
DRAWN BY : AH				
DATE REV. : 11/21/98				
REV. BY : JSB				
P.O. Box 142 Mifflintown, PA 17058-0142				



# Design Guidelines

## Bench Without Back



## Design Guidelines

### Planters

<b>BELL PLANTERS</b>				
<b>ITEM NO</b>	<b>TOP OD</b>	<b>TOP ID</b>	<b>BASE ID</b>	<b>DEPTH</b>
RB 10	10 ¼	7 5/8	5	7 ½
RB 13	13 ¼	10 3/8	6 ¾	10 1/8
RB 15	15 3/8	12 5/8	8 3/8	12 ½
RB 17	17 ½	14 ¾	9 ½	14 3/8
RB 20	20 ¼	17 3/8	11	16 ¾
RB 24	24	20	13 ¼	20
RB 30	30	25	16 ¾	24
RB 35	35	29 ½	20 ¼	25 ¼
RB 40	40	33 ¾	23 ¼	29
RB 48	48	40 ½	28 ¼	34
RB 52	52	44 ½	30 ¾	39



## RIVERSIDE PLASTICS, INC

P.O. BOX 421  
INDUSTRY ROAD  
FLEMINGSBURG, KY 41041

Phone: 606-849-3383  
Fax: 606-849-3393  
Email: [riverside-plastics@kywirelesscollc.com](mailto:riverside-plastics@kywirelesscollc.com)  
Website: [www.riverside-plastics.com](http://www.riverside-plastics.com)



Quarry Black  
3215



## Design Guidelines

### Planters Bell Planters

FLAT BOTTOM PLANTERS				
ITEM NO	TOP OD	TOP ID	BASE ID	DEPTH
FB12	12 1/2	10 5/8		3 7/8
FB16	15 1/2	13 1/2		6 1/2
FB20	19 1/2	17 1/2		8
FB24	23 1/2	21		10
FB28	27 1/2	24 1/2		13
FB34	34 1/2	31 1/2		13



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Website: [www.riverside-plastics.com](http://www.riverside-plastics.com)



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3215

## Design Guidelines

### Planters Rectangular

RECLIGHT GREENGLAR PLANTERS				
ITEM NO	TOP OD	TOP ID	BASE ID	DEPTH
REC 24	24 ¼ L x 12 ¼ W	21 ¼ L x 9 ¼ W	20 ¼ L x 8 3/8 W	10 ¾
REC 30	30 ¼ L x 12 ¼ W	27 ¼ L x 9 ¼ W	26 ¼ L x 8 3/8 W	10 ¾
REC 36	36 ¼ L x 12 ¼ W	33 ¼ L x 9 ¼ W	32 ¼ L x 8 3/8 W	10 ¾
LREC 36	36 1/4L x 15 1/4W	33 1/4L x 12 1/4W	32 1/4L x 11 3/8W	13 ¾



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Website: [www.riverside-plastics.com](http://www.riverside-plastics.com)



Quarry Black  
3215



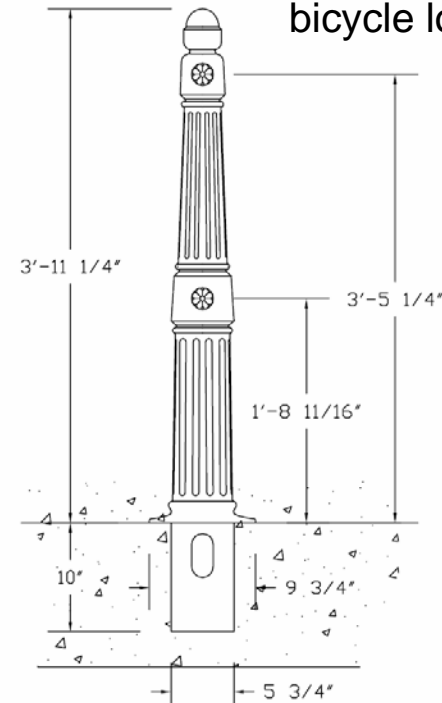
## Design Guidelines

### Bollards



## VICTORIAN

Field modify to permit  
bicycle lock down



### M9022 BOLLARD

- Available in gray iron (App. 140 lbs.) or,
- Cast aluminum per (App. 50 lbs)
- Permanent or removable installation
- With or without provision for chain
- Standard black polyurethane finish on cast iron
- Standard black powder coat on cast aluminum
- Wide range of optional colors available or custom color matched to your specification

**IRONSMITH BOLLARDS**  
FOR LANDSCAPE ARCHITECTURE

(800) 338-4766

# Design Guidelines

## Trash Receptacles Moveable



Field modify base assembly to permit stable yet flexible placement of receptacles

1/4" x 2" HORIZONTAL SOLID STEEL BAND

Ø 25"

32 1/4"

17"

(35) 3/8" x 1" VERTICAL SOLID STEEL BARS

5/8" SOLID STEEL TOP RING

(4) ADJUSTABLE FEET WITH 3/8" DIAMETER THREADED STEEL SHAFT

3/4" SQUARE INCHER ANCHOR BOLT HOLE

P.O. DRAWER 330 - DUNKIRK, MD 20754 USA  
TOLL FREE: (800) 369-2573 (USA & CANADA)  
TEL (301) 853-8300 - FAX (410) 257-7579  
WEB SITE: HTTP://WWW.VICTORSTANLEY.COM

24-GALLON CAPACITY HIGH DENSITY PLASTIC LINER (WEIGHT NOT TO EXCEED 5 LBS.) SLITS ON 3/8" x 3" SUPPORT BARS

**AVAILABLE OPTIONS:**

**POWDER COATING**  
10 STANDARD COLORS, CUSTOM COLORS (INCLUDING THE RAL RANGE).

**CUSTOM PLAQUES & DECALS**  
AVAILABLE WITH STEEL PLAQUES IN VARIOUS SIZES AND PRESSURE SENSITIVE VINYL OUTDOOR DECALS.

**LIDS**  
STANDARD TAPERED FORMED LID. AVAILABLE WITH OPTIONAL S-1 FORMED DOME, S-1 FORMED DOME WITH STAINLESS STEEL ASHTRAY, AND OS-24 FORMED DOME WITH SELF-CLOSING DOOR (AS SHOWN).

**SECURITY**  
LID IS SECURED WITH VINYL COATED GALVANIZED STEEL AIRCRAFT CABLE. CABLE IS LOOPED AROUND WELDED IN PLACE ATTACHMENT BRACKETS AND CRIMPED IN PLACE. AVAILABLE WITH OPTIONAL MOUNT WITH 3 IN-LINE ANCHOR HOLES AND OPTIONAL BOTTOM PLATE COVER.

**NOTES:**

- DRAWINGS NOT TO SCALE. DO NOT SCALE DRAWINGS.
- ALL FABRICATED METAL COMPONENTS ARE STEEL SHOTBLASTED, ETCHED, PHOSPHATIZED, PREHEATED, AND ELECTROSTATICALLY POWDER-COATED WITH T.G.T.C. POLYESTER POWDER COATINGS. PRODUCTS ARE FULLY CLEANED AND PRETREATED, PREHEATED AND COATED WHILE HOT TO FILL CREVICES AND BUILD FILM COATING. COATED PARTS ARE THEN FULLY CURED TO COATING MANUFACTURER'S SPECIFICATIONS. THE THICKNESS OF THE RESULTING FINISH AVERAGES 8-10 MILS (200-250 MICRONS).
- THIS VICTOR STANLEY, INC., PRODUCT MUST BE PERMANENTLY AFFIXED TO THE GROUND. CONSULT YOUR LOCAL CODES FOR REGULATIONS.
- VICTOR STANLEY, INC., PLASTIC INNER LINERS ARE MOLDED ON TOOLING DESIGNED FOR AND OWNED BY VICTOR STANLEY, INC. THEY OFFER MAXIMUM CAPACITY AND STRENGTH WITH LIGHTWEIGHT CONSTRUCTION USING CRITICAL MOLDED RIBS, INTEGRAL HANDHOLES, AND HIGH-STRENGTH MATERIALS. THIS MINIMIZES HANDLING DIFFICULTY AND FACILITATES EASY EMPTYING AND STORAGE WHILE AFFORDING LONG SERVICE LIFE.
- ANCHOR BOLT NOT PROVIDED BY VICTOR STANLEY, INC.
- FOR HIGH SALT ABRASIVE CLIMATES, HOT DIP GALVANIZING BEFORE POWDER COATING IS AVAILABLE. SEE WRITTEN SPECIFICATIONS FOR DETAILS.
- ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. CONTACT MANUFACTURER FOR DETAILS.
- THIS PRODUCT IS SHIPPED FULLY ASSEMBLED.

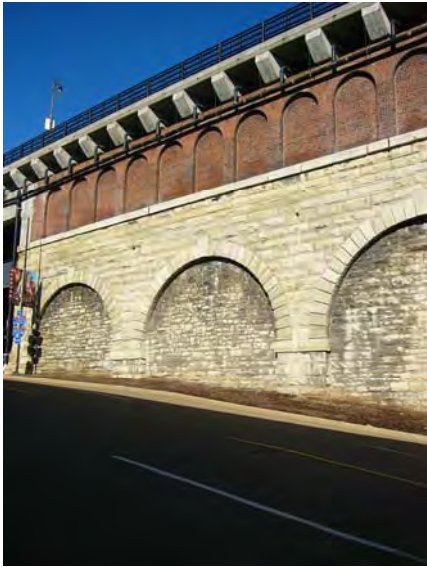
**S-35**  
TRONSTAYS® SERIES 24-GALLON LITTER RECEPTACLE  
SHOWN: OPTIONAL OS-24 FORMED DOME LID WITH SELF-CLOSING LID

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REV. 7/16/07 DRAWN M.C.W. 2007-240



Design Guidelines

**Washington Street Beautification Plan**



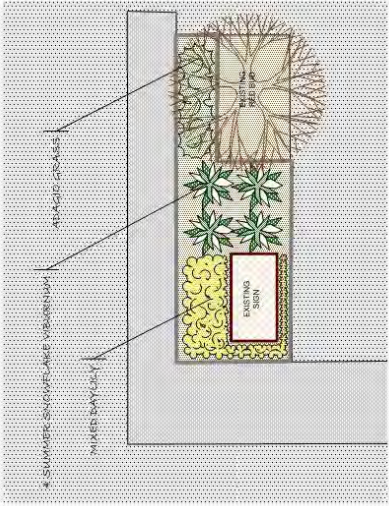
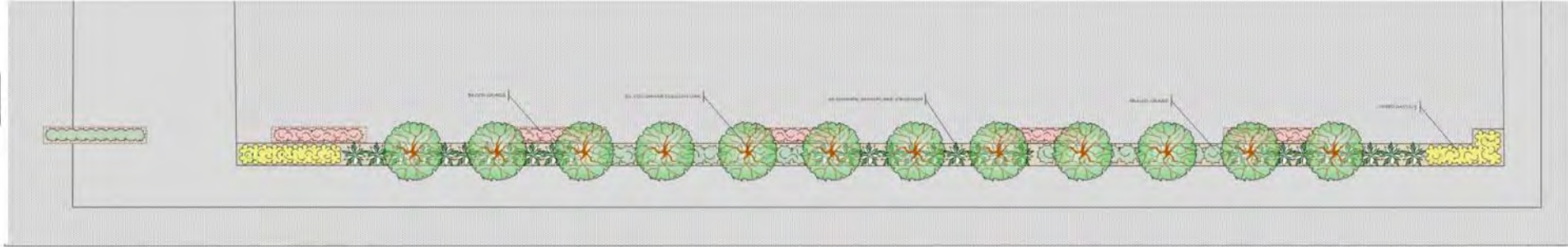




Design Guidelines

**Washington Street Beautification Plan**

South Side of Washington





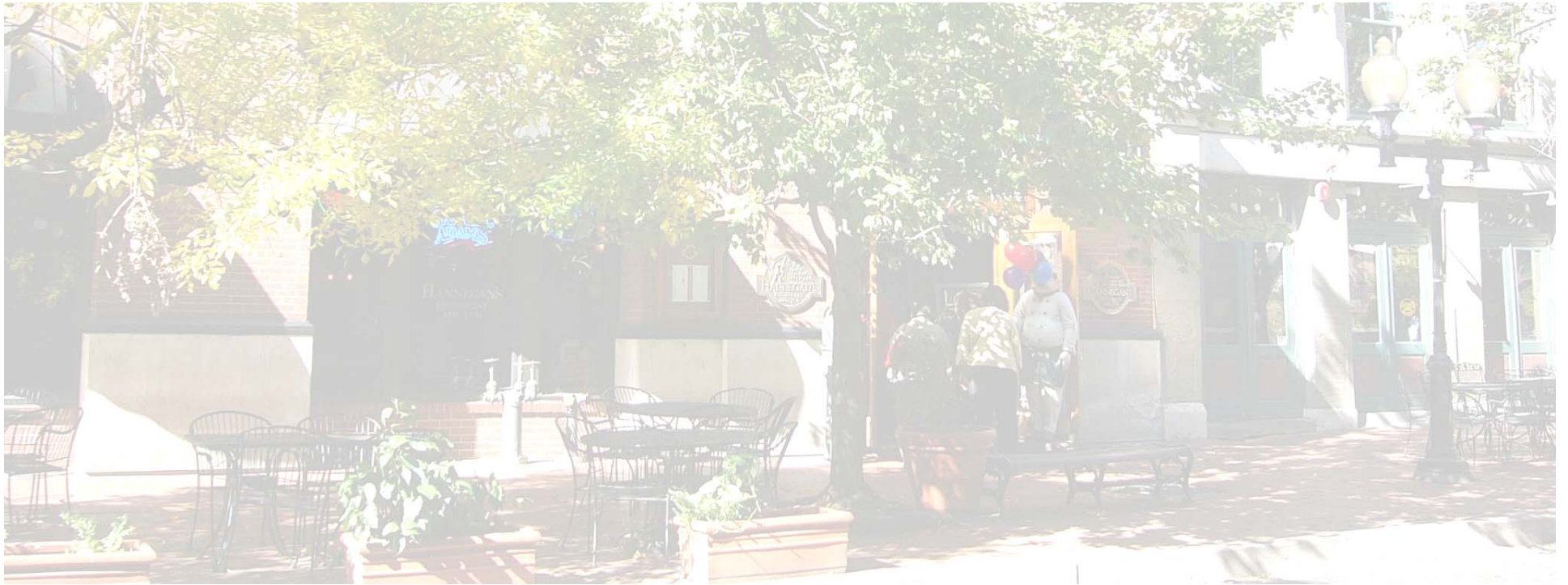
# Design Guidelines

## Washington Street Beautification Plan

### Plant Selection







**Cost**

# Cost

## Cost

**Tab #5** contains a generalized **Cost** budget level estimate for all improvements throughout the District. The first step in determining the level of effort required, consisted of measuring each block face in the District. The District contains approximately 12,5000 lineal feet (LF) of block face.

Each Block face was then classified as to its need for repair/renovation as either: (1) Maximum, (2) Moderate or (3) Minimum cost requirement. Each Classification was represented by a typical block within the District.

A list of improvements for each of the three representative blocks was prepared, quantity take-offs completed and unit cost were applied. The result was a cost of general improvements for that representative block. Each of the costs were then applied to the (previously mentioned) block face classifications and LF measurements.

The unit costs were based on mid-2009 prices and do not include escalation factors.

The total estimated cost of Public Realm Streetscape improvements is approximately \$4.17 million (does not include street improvements).



## PUBLIC REALM STREETSCAPE - Areas and Quantities

See Block Numbers Location Map

Maximum - Moderate - Minimum Cost Classification

### BLOCKS NUMBERS

Block	Block Face	Lineal Feet	Cost Class		LF	Cost Class		LF	Cost Class		
<b>68</b>	North	305	max	<b>25</b>	North	332	max	<b>16</b>	North	320	max
	East	171	max		East	158	max		East	148	max
	South	268	mod		South	331	max		South	295	mod
	West	163	mod		West	171	max		West	153	max
	Alley (15W)	172	min		Alley	0			Alley (18W)	148	min
<b>Total</b>		<b>1079</b>		<b>Total</b>		<b>992</b>		<b>Total</b>		<b>1064</b>	
<b>67</b>	North	276	mod	<b>26</b>	North	312	max	<b>15</b>	North	268	mod
	East	269	max		East	266	max		East	265	max
	South	305	mod		South	310	max		South	264	max
	West	263	mod		West	271	max		West	266	max
	Alley (22W)	268	min		Alley (20W)	268	min		Alley (20W)	267	min
<b>Total</b>		<b>1381</b>		<b>Total</b>		<b>1427</b>		<b>Total</b>		<b>1330</b>	
<b>66</b>	North	301	mod	<b>27</b>	North	322	max	<b>14</b>	North	297	max
	East	167	max		East	155	max		East	145	max
	South	311	mod		South	321	max		South	290	max
	West	156	mod		West	166	max		West	154	max
	Alley	0			Alley (18W)	161	min		Alley (18W)	150	min
<b>Total</b>		<b>935</b>		<b>Total</b>		<b>1125</b>		<b>Total</b>		<b>1036</b>	

### OTHER AREAS

	LF	Cost Class		LF	Cost Class
North 3rd Plaza	450	mod	Pedestrian Access		
Eads Bridge Portals			@Parking Lot	600	mod
N. 2nd Street	60	mod	Two Pedestrian		
Clamorgan Alley	60	mod	Plazas	80	max
N. 1st Street	60	mod			
Commercial Alley	60	mod			
EKS Boulevard	60	mod			
MLK Boulevard Park					
Opposite Block 16	250	mod			
Opposite Block 25	300	mod			
Opposite Block 68	150	mod			
<b>Total</b>	<b>1450</b>		<b>Total</b>	<b>680</b>	

### SUMMARY OF BLOCK AREAS

SUMMARY BLOCK AREAS <sup>1</sup>		
Cost Class		
Max	<b>6409</b>	LF
Mod	<b>4656</b>	LF
Min	<b>1434</b>	LF
<b>Total</b>	<b>12499</b>	LF

# PUBLIC REALM STREETSCAPE - Block Number Location

Location Map





## SUMMARY OF STREETScape COST

Cost<sup>3</sup> is based on Cost Classification Unit Cost<sup>2</sup> X LF Quantity<sup>1</sup> of Streetscape

	Cost Class		Costs <sup>3</sup>
	Total LF <sup>1</sup>	Unit Cost <sup>2</sup>	
<b>Blocks That Require:</b>			
A. <b>Maximum</b> Redevelopment (Based on Block 26-Block Face E)	6409	\$ 306.53	\$ 1,964,558.48
B. <b>Moderate</b> Redevelopment (Based on Block 15-Block Face N)	4656	\$ 212.58	\$ 989,758.83
C. <b>Minimum</b> Redevelopment (Based on Block 68-Alley)	1434	\$ 100.26	\$ 143,769.28
<b>Other Costs</b>			
D. <b>Portal Signs, Building Sign Painting, Core Intersection Treatment</b>	na	LS	\$ 400,000.00
<b>Subtotal Construction Cost</b>	12499		<b>\$ 3,498,086.59</b>
<b>Professional Services Fees</b>			
Topographic, Property & Easement Surveys			\$ 30,000.00
Geotechnical Soils Investigation - Allocation if needed			\$ 15,000.00
Design, Engineering & Construction/Bid Docs	10%		\$ 349,808.66
Design Team Construction Period Services	3%		\$ 104,942.60
Construction Management & Support Services	5%		\$ 174,904.33
<b>Subtotal Professional Services</b>			<b>\$ 674,655.59</b>
<b>Total Cost</b>			<b>\$ 4,172,742.18</b>
<i>costs are conceptual general order of magnitude and do not contain an escalation factor</i>			

## SUMMARY OF BLOCK AREAS

Cost Classification

SUMMARY BLOCK AREAS <sup>1</sup>		
Cost Class		
Max	6409	LF
Mod	4656	LF
Min	1434	LF
<b>Total</b>	12499	LF

## PUBLIC REALM STREETScape - Typical Elements

COST CLASSIFICATION<sup>2</sup>: **Maximum** Redevelopment (Based on Block 26-Block Face E)

Item	Quantity	Unit	Unit Cost	Costs	
Demolition	1	LS	1500	\$ 1,500.00	
Replace Concrete Curbs with Granite Curbs	266	LF	20	\$ 5,320.00	
Replace/Reset Signs	5	E	1000	\$ 5,000.00	
Replace Light Fixture and Pole	1	E	3000	\$ 3,000.00	
Repair Light Fixture and Pole	1	E	1000	\$ 1,000.00	
Relamp Street Lights	6	E	1500	\$ 9,000.00	
Reset Street Lights	1	E	1000	\$ 1,000.00	
				\$ -	
Replace Brick Subbase	8'x266'	2128	SF	3	\$ 6,384.00
Reset Brick Sidewalk		2000	SF	5	\$ 10,000.00
Replace Brick Sidewalk		128	SF	11	\$ 1,408.00
New Street Trees		6	E	300	\$ 1,800.00
Reset Boxes and Valves		5	E	1000	\$ 5,000.00
New Benches		3	E	1000	\$ 3,000.00
New Trash Receptacles		3	E	500	\$ 1,500.00
Drinking Fountain		0.5	E	500	\$ 250.00
Tree Boxes and Paver Edging		1	LS	4000	\$ 4,000.00
Wheel Chair Ramps		2	E	500	\$ 1,000.00
Special Conditions Access to Storefronts		120	SF	2	\$ 240.00
Sidewalk Vault Replacement		1	E	1500	\$ 1,500.00
				\$ -	
Banners/Planters/Bollards		1	LS	3000	\$ 3,000.00
Flagpole Replacement		1	E	1000	\$ 1,000.00
Decorative Lighting		1	LS	5000	\$ 5,000.00
<b>Subtotal</b>					<b>\$ 70,902.00</b>
Construction Contingency	15%				\$ 10,635.30
<b>Subtotal Construction Costs</b>					<b>\$ 81,537.30</b>
					Construction Cost
Gross Cost Per Lineal Foot	266	LF		<b>Unit Cost<sup>2</sup></b>	<b>\$ 306.53</b>
Gross Cost Per SquareFoot	2128	SF			\$ 38.35



## PUBLIC REALM STREETScape - Typical Elements

COST CLASSIFICATION<sup>2</sup>: *Moderate* Redevelopment (Based on Block 15-Block Face N)

Item	Quantity	Unit	Unit Cost	Costs	
Demolition	1	LS	1000	\$ 1,000.00	
Replace Concrete Curbs with Granite Curbs	266	LF	20	\$ 5,320.00	
Replace/Reset Signs	5	E	1000	\$ 5,000.00	
Replace Light Fixture and Pole	1	E	2500	\$ 2,500.00	
Repair Light Fixture and Pole	1	E	1000	\$ 1,000.00	
Relamp Street Lights	6	E	1000	\$ 6,000.00	
Reset Street Lights	1	E	1000	\$ 1,000.00	
				\$ -	
Replace Brick Subbase	6'X266'	1560	SF	2.75	\$ 4,290.00
Reset Brick Sidewalk	1500	SF	5	\$ 7,500.00	
Replace Brick Sidewalk	60	SF	11	\$ 660.00	
New Street Trees	4	E	300	\$ 1,200.00	
Reset Boxes and Valves	5	E	1000	\$ 5,000.00	
New Benches	1	E	1000	\$ 1,000.00	
New Trash Receptacles	1	E	500	\$ 500.00	
Drinking Fountain	0	E	500	\$ -	
Tree Boxes and Paver Edging	1	LS	3000	\$ 3,000.00	
Wheel Chair Ramps	2	E	500	\$ 1,000.00	
Special Conditions Access to Storefronts	100	SF	2	\$ 200.00	
Sidewalk Vault Replacement	1	E	1500	\$ 1,500.00	
				\$ -	
Banners/Planters/Bollards	1	LS	1500	\$ 1,500.00	
Flagpole Replacement	0	E	1000	\$ -	
Decorative Lighting	0	LS	5000	\$ -	
Subtotal				\$ 49,170.00	
Construction Contingency	15%			\$ 7,375.50	
Subtotal Construction Costs				\$ 56,545.50	
				Construction Cost	
Gross Cost Per Lineal Foot	266	LF	Unit Cost <sup>2</sup>	\$ 212.58	
Gross Cost Per SquareFoot	2126	SF		\$ 26.60	

## PUBLIC REALM STREETScape - Typical Elements

COST CLASSIFICATION<sup>2</sup>: **Minimum** Redevelopment (Based on Block 68-Alley)

Item	Quantity	Unit	Unit Cost	Costs
Demolition	1	LS	500	\$ 500.00
Replace Concrete Curbs with Granite Curbs	0	LF	20	\$ -
Replace/Reset Signs	0	E	1000	\$ -
Replace Light Fixture and Pole	1	E	2500	\$ 2,500.00
Repair Light Fixture and Pole	0	E	1000	\$ -
Relamp Street Lights	0	E	1000	\$ -
Reset Street Lights	0	E	1000	\$ -
				\$ -
Replace Brick Subbase	1000	SF	2.75	\$ 2,750.00
Reset Brick Sidewalk	1000	SF	5	\$ 5,000.00
Replace Brick Sidewalk	0	SF	11	\$ -
New Street Trees	4	E	300	\$ 1,200.00
Reset Boxes and Valves	5	E	1000	\$ 5,000.00
New Benches	0	E	1000	\$ -
New Trash Receptacles	0	E	500	\$ -
Drinking Fountain	0	E	500	\$ -
Tree Boxes and Paver Edging	0	LS	3000	\$ -
Wheel Chair Ramps	2	E	500	\$ 1,000.00
Special Conditions Access to Storefronts	120	SF	2	\$ 240.00
Sidewalk Vault Replacement	0	E	1500	\$ -
				\$ -
Banners/Planters/Bollards	0	LS	1500	\$ -
Flagpole Replacement	0	E	1000	\$ -
Decorative Lighting	1	LS	5000	\$ 5,000.00
				\$ -
Subtotal				\$ 23,190.00
Construction Contingency	15%			\$ 3,478.50
Subtotal Construction Costs				\$ 26,668.50
				Construction Cost
Gross Cost Per Lineal Foot	266	LF	Unit Cost <sup>2</sup>	\$ 100.26
Gross Cost Per SquareFoot	2126	SF		\$ 12.54